

TOWN OF LOS GATOS
NORTH 40 SPECIFIC PLAN
JUNE 17, 2015



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NORTH40 SPECIFIC PLAN

Adopted By:

Resolution XXX and Ordinance XXX

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1. INTRODUCTION

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1. INTRODUCTION

1.1 PURPOSE

The purpose of the North 40 Specific Plan is to implement the Town of Los Gatos General Plan and to comprehensively plan for future development in the Specific Plan Area. The Specific Plan will be a regulatory tool that the Town of Los Gatos will use to guide future development. While the General Plan is the primary guide for growth and development in Los Gatos, the Specific Plan focuses on the unique characteristics of the Specific Plan Area and customizes the planning process and land use regulations to reflect the Town Vision Statement and Guiding Principles for this area.

The Vision Statement was inspired by community input through Advisory Committee meetings, Community Workshops, and Town Council/Planning Commission study sessions.

The Specific Plan provides a vision for a unique new neighborhood that will incorporate the site's unique agricultural characteristics. It establishes appropriate land uses, development standards, urban design concepts, and architectural design guidelines for the Specific Plan Area, which are combined with an organized structure for circulation, parking and infrastructure.

The intent of this Specific Plan is to provide a comprehensive framework in which development can occur in a planned, logical fashion rather than a piecemeal approach. This is an important aspect due to the multiple land ownerships that presently exist within the boundaries of the Specific Plan Area. This comprehensive approach will help to create a unified new neighborhood for Los Gatos residents to enjoy and will better serve North Los Gatos residents, employees, and businesses.

VISION STATEMENT

The North 40 reflects the special nature of our hometown. It celebrates our history, agricultural heritage, hillside views, and small town character. The North 40 is seamlessly woven into the fabric of our community, complementing other Los Gatos residential and business neighborhoods. It is respectful of precious community resources and offers unique attributes that enrich the quality of life of all of our residents.

Guiding Principles to achieve this vision:

- The North 40 will look and feel like Los Gatos.
- The North 40 will embrace hillside views, trees, and open space.
- The North 40 will address the Town's residential and/or commercial unmet needs.
- The North 40 will minimize or mitigate impacts on town infrastructure, schools, and other community services.

Adopted by Town Council March 5, 2012.

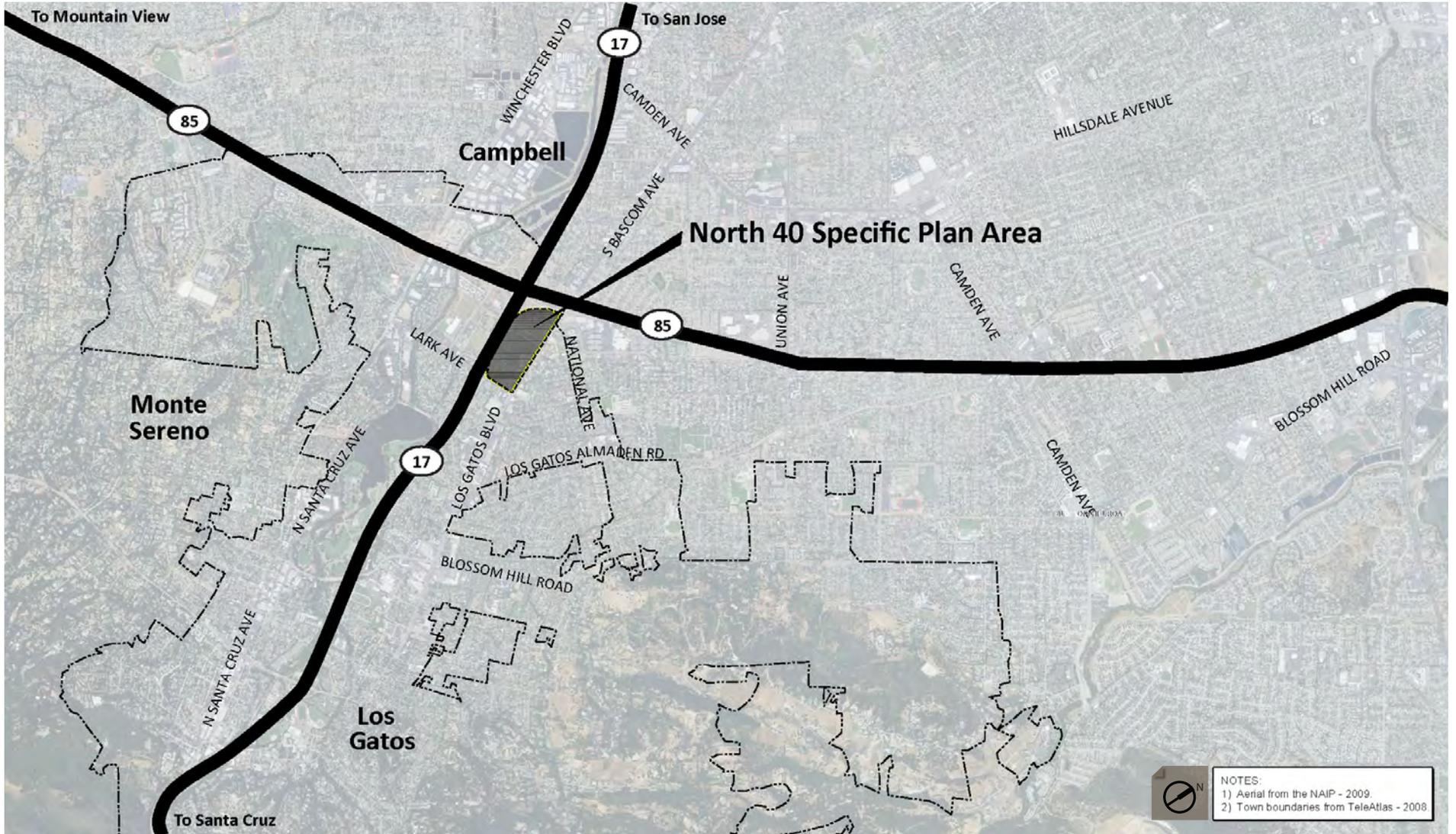


Figure 1-1 Context Map

1.2 BACKGROUND

The Specific Plan was originally drafted in 1999 but never adopted. In 2010, the Los Gatos General Plan was updated and included goals, policies, and general guidelines for the Specific Plan Area in the Land Use Element. In addition, the General Plan's Vasona Light Rail Element outlines policies and implementation strategies for the Specific Plan Area and Los Gatos Boulevard.

1.2.1 CONTEXT

The Town of Los Gatos lies at the base of the Santa Cruz Mountains. It is approximately 60 miles south of San Francisco in Santa Clara County. It is bounded by the City of San Jose to the north and east, the City of Campbell to the north, the City of Monte Sereno and the City of Saratoga to the west, and the unincorporated areas of the County of Santa Clara to the south. Although the Town is situated within one of the largest metropolitan areas of Northern California and is closely tied to the fast paced economy of Silicon Valley, the Town still retains its small town character. Los Gatos provides a rich community fabric composed of a varied mix of residential, commercial, and light industrial uses that serve the residents and attracts visitors. The Specific Plan Area is located at the northern gateway to the Town of Los Gatos with the Santa Cruz Mountains providing a backdrop a few miles to the south. The Specific Plan Area is in close proximity to the employment center of Good Samaritan Hospital and other medical facilities as well as existing residential neighborhoods. The Specific Plan Area will serve the need of these adjacent uses.

1.2.2 SPECIFIC PLAN BOUNDARY

The Specific Plan Area is bounded by Los Gatos Boulevard to the east, State Route 17 (Highway 17) to the west, Lark Avenue to the south, and State Route 85 (Highway 85) to the north. It is approximately 44 acres and currently consists of commercial developments along portions of Los Gatos Boulevard, pockets of existing residential uses along Los Gatos Boulevard and within the Specific Plan Area along Bennett Way and Burton Road, and former agriculture uses contained in the areas interior. There are residential neighborhoods, commercial uses, and office buildings to the south and east of the Specific Plan Area. The Specific Plan Area is largely a flat plane gently sloped from southeast to northwest, depressed an average of 10 feet below Los Gatos Boulevard. While access from Lark is nearly level, there is an embankment with slopes of 5% to 14% along the length of Los Gatos Boulevard.



Figure 1-2 Location Map



Figure 1-3 Existing Conditions

1.3 EXISTING CONDITIONS

1.3.1 LAND USE

The Specific Plan Area consists of a mix of existing land uses including residential and commercial uses along Los Gatos Boulevard. There is also agricultural land that is designated as “Unique Farmland” within the Specific Plan Area. The “Unique Farmland” classification, applied to the orchards on the site, is a designation applied by the State Department of Conservation to land that is capable of producing the state’s leading crops. The Specific Plan Area formerly produced walnut and fruit crops. Land uses in the vicinity of the Specific Plan Area consist primarily of mixed commercial, neighborhood shopping centers, automobile sales and repair, service stations, office, and residential uses. The 2020 General Plan land use designation is Mixed Use Commercial and the General Plan provides for a Specific Plan Overlay Designation on the site for mixed-use commercial and residential uses. The current zoning includes R-1 (Single-Family Residential), CH (Restricted Commercial Highway), and RC (Resource Conservation).

1.3.2 CIRCULATION

The site is bounded by two main arterials: Lark Avenue and Los Gatos Boulevard. Lark Avenue provides access to Highway 17 on the southwest boundary of the site and connects the northwest side of Los Gatos with the future Vasona Light Rail station. Los Gatos Boulevard provides direct access to Highway 85 on the northeast end of the site as well as connections to Downtown and other services in Los Gatos to the south. Traffic signals are located at four intersections on or near the boundary of the Specific Plan Area: Los Gatos Boulevard and the on/off ramps of Highway 85; Los Gatos Boulevard and Burton Road/Samaritan Drive; Los Gatos Boulevard and Lark



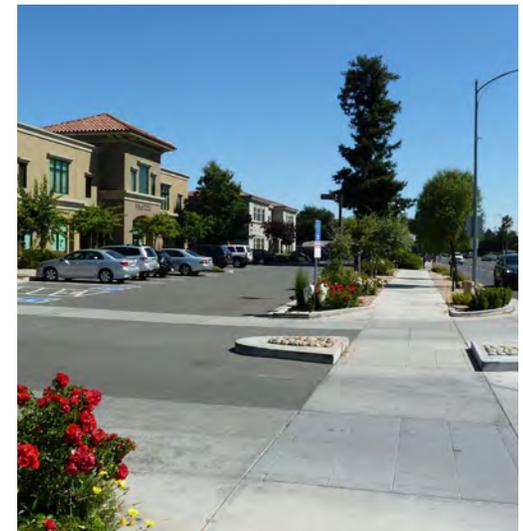
Existing barn and orchard.



76 Station, corner of Los Gatos Boulevard and Lark Avenue.



Los Gatos Boulevard, looking North.



Existing commercial along Los Gatos Boulevard.

Avenue; and Lark Avenue and the on/off ramp of Highway 17. Santa Clara Valley Transportation Authority provides public transit services to the greater Santa Clara County, with service connections at two locations on Los Gatos Boulevard abutting the Specific Plan Area. The 2020 General Plan designates Class II bicycle routes on Los Gatos Boulevard and Lark Avenue, but they currently do not exist.

1.3.3 MARKET DEMAND

A Market Study and Business Opportunities Assessment was prepared for the Town of Los Gatos by BAE Urban Economics in August 2011. The report can be found in Appendix A. This report provided a demographic and economic overview of retail, office, hotel and conference/meeting space along with market assessments with leakage and injection analysis and strategies for redevelopment. The report helped inform the vision, principles, and appropriate land uses for the Specific Plan Area by focusing on unmet needs in Los Gatos.

1.4 RELATIONSHIP TO EXISTING PLANNING DOCUMENTS

The Town of Los Gatos 2020 General Plan is the fundamental planning policy document for the Town of Los Gatos. It guides planning, environmental and land use decisions town-wide. The Specific Plan is consistent with and furthers the objectives of the General Plan adopted on September 20, 2010.

Listed below are excerpts from the General Plan's Land Use Element and Vasona Light Rail Element that specifically address the Specific Plan Area.

California law requires a Specific Plan to be consistent with a Town's General Plan. Although amendments to the Town's General Plan will be necessary to adopt this Specific Plan, the policies, standards and guidelines of the North 40 Specific Plan are consistent with the overarching goals of the Los Gatos General Plan. The General Plan Overlay Designation guidelines and Policy LU-11.4 will be amended as shown on the following page.

Los Gatos 2020 General Plan

Overlay Designation

The North Forty Specific Plan Overlay is applied to the approximately 40-acre property bounded by Los Gatos Boulevard to the east, Highway 17 to the west, Lark Avenue to the south, and Highway 85 to the North. This Overlay requires the preparation and adoption of a specific plan that will determine the mix of uses, dimensional standards, architectural standards, phasing, and infrastructure to support the development of the property prior to approval of any entitlements. Drawing on the draft North Forty Specific Plan prepared in 1999, the General Plan Overlay designation is intended to help guide the future development of this property. The 2020 General Plan Environmental Impact Report assumed a maximum capacity of 750 mixed residential units and 580,000 square feet of retail and offices uses for the purposes of assessing environmental impacts associated with the development of the property. While this is the maximum development capacity under this General Plan, the specific plan may be approved with lower densities and square footage of residential and commercial uses, respectively. The North Forty Specific Plan will be based on the following general guidelines:

- ◆ *Include a mixture of uses that will complement the Downtown and the rest of the community*
- ◆ *Be based on sustainable and "smart" development practices*
- ◆ *Include public gathering spaces such as a plaza and park*
- ◆ *Provide for a variety of residential housing types, both rental- and owner-occupied. A minimum of 20 % of the units shall be affordable to households at the moderate income level or below*
- ◆ *Include high-quality architecture and design that reflects the rural and agricultural history of the site*
- ◆ *Provide pedestrian-oriented buildings along the Los Gatos Boulevard frontage, with minimal parking oriented to the street*
- ◆ *Take advantage of the grade change across the site*

- ◆ Continue the “boulevard treatment” along the southwestern end of Los Gatos Boulevard, with interconnections from one parcel’s drive aisle to the next where feasible. [General Plan Amendment Required]
- ◆ Include connections to existing intersections along Los Gatos Boulevard and Lark Avenue
- ◆ Allow for future development of gateway or landmark features at Los Gatos Boulevard and Lark Avenue and Los Gatos Boulevard and the Highway 85 off-ramp [General Plan Amendment Required]
- ◆ Provide an easily accessible, fully connected street network that encourages walking
- ◆ Provide a vegetative buffer and screening along Highway 17 and 85
- ◆ Preserve Town character and views

1.5 GENERAL PLAN GOALS, POLICIES, AND ACTIONS

The Los Gatos 2020 General Plan has Goals, Policies, and Actions that guide development within the Town of Los Gatos. The following General Plan Goals, Policies, and Actions are specific to the Specific Plan Area.

<i>Goal LU-4</i>	<i>To provide for well-planned, careful growth that reflects the Town’s existing character and infrastructure.</i>
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Policies

<i>Policy LU-4.1</i>	<i>Integrate planning for the North Forty area, Los Gatos Boulevard, Vasona Light Rail area, and Downtown so that development in each area takes into consideration the Town as a whole.</i>
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<i>Goal LU-11</i>	<i>To Plan for development of a variety of uses in the North Forty area in a coordinated and comprehensive way.</i>
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Policies

- | | |
|-----------------------|---|
| <i>Policy LU-11.1</i> | <i>Zoning shall be changed as part of the development applications to provide consistency with the Vasona Light Rail Element and other elements of this General Plan and with any future specific plan prepared for this area.</i> |
| <i>Policy LU-11.2</i> | <i>The Town shall encourage uses that serve Town residents. These include, but are not limited to open space, playfields, office, retail, and other commercial uses. Residential uses may be permitted as part of mixed-use development and only with acceptable mitigation of adverse noise, air quality, and other environmental hazards.</i> |
| <i>Policy LU-11.3</i> | <i>Provide coordinated infrastructure in the North Forty area.</i> |
| <i>Policy LU-11.4</i> | <i>Include a variety of commercial uses reflective of Town Council’s Vision and Guiding Principles for the North Forty. Land uses shall follow a logical land use pattern that takes advantage of the site opportunities while protecting adjacent uses. [General Plan Amendment Required]</i> |
| <i>Policy LU-11.5</i> | <i>Avoid negative effects on the long-term development potential of the area surrounding the North Forty area.</i> |
| <i>Policy LU-11.6</i> | <i>Incorporate multimodal links from the North Forty area to the Vasona Light Rail station into the North Forty Specific Plan.</i> |

Action

Action LU-11.1 Prepare and adopt a specific plan for the North Forty area prior to development of the site.

Goal VLR-3 To encourage mixed-use developments that coordinate housing in proximity to either neighborhood commercial uses or employment centers.

Policies

Policy VLR-3.4 Encourage mixed-use development of commercial, office, and medium-high residential uses in the North Forty area and along East Los Gatos Boulevard, north of Lark Avenue.

Goal VLR-9 To reduce traffic impacts of residential development within the Vasona Light Rail area by taking advantage of mass transit opportunities.

Policies

Policy VLR-9.5 Promote the development of mass transit links between Los Gatos Boulevard, particularly any development on the North Forty site, and the planned Vasona Light Rail station.

Goal CD-1 Preserve and enhance Los Gatos' character through exceptional community design.

Policies

Policy CD-1.6 Town staff shall review properties next to community entry points when they are developed or redeveloped to reflect the gateway concept.

1.5.1 ZONING REGULATIONS

The Los Gatos Zoning Ordinance is the primary document that implements the General Plan. It provides regulations regarding permitted land uses, development standards, and the development entitlement process for parcels of land in the Town of Los Gatos.

The North 40 Specific Plan is also a regulatory document which provides development regulations. The requirements of this Specific Plan supplement and in some cases supersede the Zoning Ordinance within the Specific Plan boundary.

The Specific Plan contains both development regulations and design guidelines. Mandatory regulations are denoted by the use of the word “shall.” A guideline, which is denoted by the use of the word “should,” is not mandatory, but is encouraged with some discretion by staff, the Planning Commission, and the Town Council.

1.5.2 LOS GATOS BOULEVARD PLAN

The Los Gatos Boulevard Plan (Boulevard Plan) contains architectural and site design guidelines related to the commercial revitalization of Los Gatos Boulevard. The Boulevard Plan is based on a land use concept for which mixed-use commercial development in the northern portion of the Boulevard Plan Area transitions into residential and community clusters, and then into existing residential neighborhoods south of the Boulevard Plan Area. The Boulevard Plan contains policies and design standards targeting safe, attractive public improvements, pedestrian-oriented activity nodes, commercial land uses, distinct gateways, and site-appropriate private improvements.

The Specific Plan incorporates and/or complements the concepts and guidelines from the Los Gatos Boulevard Plan where applicable.

1.5.3 COMMERCIAL DESIGN GUIDELINES

Los Gatos has Commercial Design Guidelines that focus on commercial development within the Town and within four specific districts near Downtown and along Los Gatos Boulevard between Los Gatos/Saratoga Road (Highway 9) and Lark Avenue. The relevant design guidelines from the Commercial Design Guidelines document have been incorporated into the Specific Plan. The Specific Plan standards and guidelines supersede the existing Los Gatos Commercial Design Guidelines for development within the Specific Plan Area.

1.5.4 AFFORDABLE HOUSING OVERLAY ZONE AND DESIGN GUIDELINES

The Affordable Housing Overlay Zone (AHOZ) accommodates for affordable housing for owner occupied and/or rental for low, very low, and extremely low income households.

While these guidelines do not currently apply to the Specific Plan Area, relevant guidelines have been carried forward and are contained within this Specific Plan. The adoption of the Specific Plan does not preclude future possibilities of the Town identifying portions of the site as an Affordable Housing Overlay Zone.

1.6 OUTREACH

The public participation process was an integral aspect of the North 40 Specific Plan. This effort built upon many of the components of the previous 1999 Draft North Forty Specific Plan and was guided by a Town Council appointed North 40 Advisory Committee made up of 13 Town Council, Planning Commission, and community representatives. The committee participated in over 17 meetings, over a two year period, to provide direction that was consistent with the Town's vision and the basis for this Specific Plan. Topics included development capacity and thresholds, supporting land uses, development standards, and guidelines. In addition to Advisory Committee meetings, the outreach process also included public participation through two community workshops, an on-line survey, and three Town Council/Planning Commission study sessions. The Town Council, Planning Commission, Advisory Committee, and community input were the foundation and inspiration for this Specific Plan.

1.7 WHAT IS A SPECIFIC PLAN?

A Specific Plan is a document designed to implement the goals and policies of the General Plan. It contains detailed development standards and regulations, distribution of land uses, infrastructure requirements, and implementation measures for the development of a specific geographic area.

The Specific Plan is similar in nature to the Zoning Ordinance because it deals with implementation through the use of development regulations. Unlike the town-wide zoning ordinance, the Specific Plan targets a specific area, the North 40. This allows for tailored regulations and standards to achieve desired strategies and to create quality development. These provisions also provide a greater level of assurance to prospective developers and land owners relative to the Town's long-term goals for the Specific Plan Area.

It is important to note that a Specific Plan only establishes land uses and development standards. Actual development proposals, building placement, and design will come through private investment following the adoption of the Specific Plan.

1.8 DOCUMENT ORGANIZATION

This Specific Plan is organized into six chapters. The following describes the contents of each chapter in more detail:

1. Introduction

The introduction discusses the purpose, background, and organization of this document. It describes the Specific Plan Area boundaries and the community outreach process, as well as the Specific Plan's relationship to the General Plan and other pertinent Town documents. The chapter also includes a brief summary of the existing conditions within the Specific Plan Area on topics such as existing land uses and circulation.

2. Land Use and Development Standards

This chapter includes the Council-adopted Vision Statement and Guiding Principles for the Specific Plan. It expands on the concepts and elements that support the Town Council's Vision Statement and Guiding Principles such as integration of orchard concepts, walkable neighborhoods, providing for uses that respond to unmet needs in the community, and the Los Gatos town character.

This chapter defines the Specific Plan Land Uses and defines the regulations that will control the size and quantity of development. It includes the Land Use Goals and Policies, land use districts, Permitted Land Use Table, Maximum Development Capacity Table (maximum

square footage/units), and development standards (height, perimeter and building setbacks, lot coverage and open space requirements, etc.) to guide future development within the Specific Plan Area.

3. Design Guidelines

The Design Guidelines chapter outlines design guidelines for future development that is reflective of the North 40 Vision Statement and Guiding Principles. Guidelines organization:

- Commercial Design Guidelines - Site planning, outdoor spaces, parking, architecture, landscaping, sustainability
- Residential Design Guidelines - Site planning, parking, open space, landscaping, architecture, sustainability
- Neighborhood Identity - Character and identity unifying elements, street furnishings, streetscape landscaping
- Signage - Entry features, tenant signage, signage types

4. Circulation and Streetscape

This chapter focuses on pedestrian, bicycle, and vehicular circulation. It contains goals, policies and street sections for internal and adjacent street design (including right-of-way dimensions and desired street and sidewalk characteristics), pedestrian connections, and transit and bicycle strategies.

5. Infrastructure and Public Facilities

This chapter summarizes the existing and needed infrastructure and public facilities in the Specific Plan Area. Topics include water supply, stormwater collection and conveyance, wastewater, utilities, solid waste, police and fire protection and schools. This chapter gives an overview of necessary upgrades to the existing infrastructure system that will be needed to support the redevelopment of the Specific Plan Area and to meet the Town goals.

6. Plan Implementation, Phasing, and Administration

This chapter outlines proposed phasing, financing, and implementation strategies. The implementation plan serves as a road map for the private sector and the Town of Los Gatos alike for plan implementation.

The chapter also includes the administration section which describes the authority of the Specific Plan and includes processing requirements and the administrative procedures required for amendments and/or modifications to the Specific Plan.

At the end of this chapter is the Glossary of key terms used in this Specific Plan.

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2. LAND USE AND DEVELOPMENT STANDARDS

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2. LAND USE AND DEVELOPMENT STANDARDS

This chapter provides direction for future development within the Specific Plan Area through regulatory tools. These tools will help achieve the overall North 40 Specific Plan vision and, more specifically, provide refined direction for the types of uses that should occur and how these uses will be allowed to develop. The Specific Plan accommodates a range of pedestrian-oriented commercial, residential, and hospitality uses while establishing associated development standards to create an inviting, walkable neighborhood. The overarching goals are to ensure future development is compatible with surrounding areas, complements Downtown Los Gatos, and contributes to the small town charm of Los Gatos. The Specific Plan Area development is intended to result in a mixed-use neighborhood that supports the nearby residential neighborhoods, adjacent businesses, hospitals, and offices, as well as, on-site residents.

Land uses and development standards presented in this chapter form a comprehensive set of policies that will work in concert to steer future development and reinforce the desired North 40 vision. The policies and regulations contained in this chapter closely work together with the policies and guidelines in Chapter 3 – Design Guidelines and in Chapter 4 – Circulation and Streetscape. A district-based approach is applied to tailor policies and recognize unique attributes and objectives for different areas of the site. Three planning districts are established, each with its own distinctive character and set of recommendations, with transition areas proposed along district boundaries to provide smooth transitions between districts. In addition, allowable land uses are defined to support the North 40 vision and ensure compatibility with surrounding areas as well as internally within the Specific Plan Area. Specific site development standards, such as building height, setbacks, and parking requirements will help create the appropriate scale and character of the envisioned development.

This chapter is organized into the following sections:

- Council Vision
- Land Use Goals and Policies
- Land Use Districts
- Permitted Land Uses
- Area-Wide Development Regulations
- Non-Residential/ Mixed-Use Development Standards
- Residential Development Standards

2.1 COUNCIL VISION

On March 5, 2012, the Los Gatos Town Council established the following Vision Statement for the Specific Plan Area.

Vision Statement:

The North 40 reflects the special nature of our hometown. It celebrates our history, agricultural heritage, hillside views, and small town character. The North 40 is seamlessly woven into the fabric of our community, complementing other Los Gatos residential and business neighborhoods. It is respectful of precious community resources and offers unique attributes that enrich the quality of life of all of our residents.

Guiding Principles to achieve this vision:

- The North 40 will look and feel like Los Gatos.
- The North 40 will embrace hillside views, trees, and open space.
- The North 40 will address the Town's residential and/or commercial unmet needs.
- The North 40 will minimize or mitigate impacts on town infrastructure, schools, and other community services.

2.2 LAND USE GOALS AND POLICIES

This section establishes land use policies formulated to guide the implementation of the Specific Plan vision. The land use policies provide direction for the development regulations and permitted uses found later in this chapter. In addition, proposed individual development projects must be consistent with the following land use policies.

Goal: *To provide land uses within the North 40 Specific Plan Area that are consistent with the Town of Los Gatos General Plan, the Town Council Vision Statement and Guiding Principles and the goals and policies set forth herein.*

Land Use Policies

Policy LU1: Land Use Designations

The Specific Plan shall be implemented through the approval of development projects that are consistent with the land uses and the Council Vision as outlined in this chapter.

Policy LU2: Specific Plan Amendments

Specific Plan Amendments must be consistent with the Vision Statement and Guiding Principles and the goals and policies set forth herein.

Policy LU3: Mix of Uses

Provide a mix and size of uses to promote the creation of a lively, walkable neighborhood that makes the North 40 Specific Plan Area a resource to North 40 residents, businesses and adjacent neighborhoods.

Policy LU4: Maximum Commercial Development

Commercial development within the Specific Plan Area shall be complementary to Downtown through the careful control of uses and permitted square footage as set forth in the Maximum Development Capacity Table (refer to Table 2-2.)

Policy LU5: Building Height

Building heights within the Specific Plan Area shall be consistent with the Specific Plan Development Standards.

Policy LU6: Retail

Retail uses within the Specific Plan Area are intended to serve North 40 residents, adjacent neighborhoods, nearby employment centers and the unmet needs of the Town of Los Gatos.

Policy LU7: Restaurants

Eating and drinking establishments within the Specific Plan Area are intended to serve North 40 residents, adjacent neighborhoods and nearby employment centers.

Policy LU8: Hotel/Conference Facilities

A hotel facility developed within the North 40 Specific Plan Area should include a conference/meeting space to accommodate 200 to 250 people.

Policy LU9: Maximum Residential Development

The maximum number of residential units will be limited as set forth in the Maximum Development Capacity Table (refer to Table 2-2.)

Policy LU10: Residential

Provide and integrate a mix of residential product types designed to minimize impacts on schools, while complying with Senate Bill 50, Schools Facilities Act and serve the unmet housing needs within the Town of Los Gatos.

Policy LU11- Economic Balance

Proposed uses should complement the existing balance and diversity of businesses located along Los Gatos Boulevard and in Downtown Los Gatos.

2.3 LAND USE DISTRICTS

The Specific Plan presents a district-based approach that promotes an integrated, local-serving neighborhood consistent with the General Plan and the North 40 vision. A district-based approach is applied in the Specific Plan. In contrast to single-use zoning based on land use type, the Specific Plan is organized by geographic areas called Planning Districts. In the district approach, each district has a distinct character and a tailored range of uses and development standards that support the desired future condition. The Specific Plan divides the North 40 into three districts based on site context and desired development characteristics. The three districts are the Lark District, Transition District and Northern District, as illustrated in Figure 2-1.

The overall goal is for all three districts to work together as a self-sufficient neighborhood while offering support services and entertainment for the local area.

In addition to the Districts, the Specific Plan Area contains two Specific Plan Overlay Zones: Perimeter Overlay Zone and District Edge Overlay Zone. The Perimeter Overlay Zone provides additional height limitations, setbacks and development standards to allow view corridors of the surrounding hillsides and weave the North 40 neighborhood together with adjacent neighborhoods. In addition, there is a District Edge Overlay Zone that is intended to provide flexibility at the district boundaries by allowing uses within each of the adjoining districts to be permitted within this area.

2.3.1 LARK DISTRICT

Located at the northeast corner of Los Gatos Boulevard and Lark Avenue, the Lark District is in close proximity to existing residential neighborhoods. Lower intensity residential and limited retail/office uses are envisioned for this area. In an effort to satisfy the Town's unmet needs, development standards have been tailored to guide the development of residential product types including multi-family, townhomes, and "cottage cluster/garden cluster" housing types. Cottage cluster housing is generally characterized by detached cottages oriented onto common greens and will be considered with a Conditional Use Permit. Garden cluster housing is characterized by attached units clustered around a common green. Envisioned land use categories include:

- Limited retail, personal services, and restaurants (primarily along Los Gatos Boulevard)
- Office (along Los Gatos Boulevard)
- Open space (pocket parks, paseos, orchard trees, and planting strips)
- Residential – including condominium, cottage cluster/garden cluster housing, rowhouses and townhomes

2.3.2 TRANSITION DISTRICT

Located in the central portion of the Specific Plan Area, the Transition District provides a transition and buffer between the lower intensity, primarily residential, character of the Lark District and the active retail and entertainment emphasis of the Northern District. The Transition District will accommodate a range of uses including neighborhood-serving stores, specialty market and mixed-use housing with residential units above commercial. A hotel or hospitality use could also be part of the land use mix. Envisioned land use categories include:

- Hotel
- Market hall/specialty market
- Office
- Open space (pocket parks, courtyards, paseos, plazas, and planting strips)
- Personal service
- Residential – including condominium, live-work flats, multi-family flats, multiplexes, rowhouses and townhomes
- Restaurants
- Retail (primarily neighborhood serving)

2.3.3 NORTHERN DISTRICT

The Northern District is bordered on two sides by State Route 17 and 85 (Highway 17 and 85) with Los Gatos Boulevard located on the east side. This condition makes it best suited for a day-to-evening entertainment area that offers shopping and restaurants for nearby residents as well as employment centers. The Northern District focuses on walkability, public spaces and amenities. Envisioned land use categories include:

- Recreation/entertainment (live theater, health clubs)
- Hotel
- Market hall/specialty market
- Office
- Open space (plazas, courtyards, paseos, and planting strips)
- Residential (above commercial) - including condominium, live-work flats, multi-family flats, multiplexes, and rowhouses
- Restaurants
- Retail

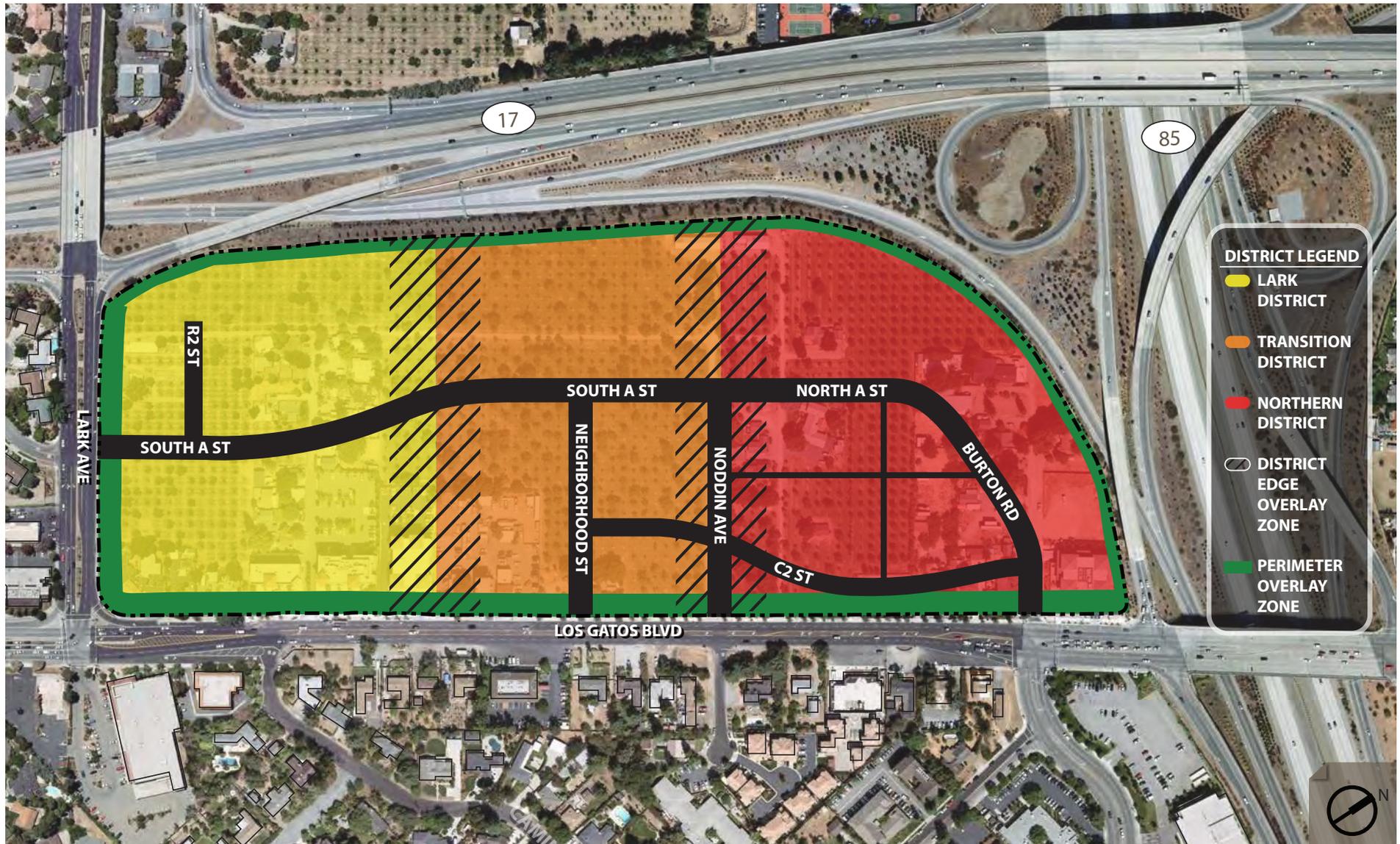


Figure 2-1 District Plan

2.4 PERMITTED LAND USES

The Specific Plan land uses help to create a pedestrian-oriented and interactive environment that is compatible with surrounding neighborhoods as well as provides for on-site uses that are compatible with each other. The Specific Plan specifies the desired mix, as well as the location of land uses. In general, lower intensity shops, offices, and residential land uses are envisioned in the southern portion of the Specific Plan Area. Moving northward, potential land uses transition to mixed-use residential and potential hospitality uses to provide a buffer between primarily residential uses in the southern portion of the Specific Plan Area and the entertainment, restaurant, and shopping uses envisioned in the northern portion of the Specific Plan Area. The Specific Plan establishes two primary land use categories to guide development within the Specific Plan Area. These land uses categories are: (1) non-residential – including retail, office, recreation, community services, restaurants, entertainment, and hotel, and (2) residential – including condominium, cottage cluster/garden cluster housing, live-work flats, multi-family flats, multiplexes, rowhouses and townhomes. Residential development is focused on multi-family housing types and shall be designed to attract the unmet housing needs of the community. Within the Transition District, the Specific Plan encourages the development of residential units over commercial development. It is important to provide an appropriate amount of residential development in the Specific Plan Area to create a sustainable and pedestrian-oriented mixed-use environment. Although residential uses will be an important component to the success of the Specific Plan Area, it will support the predominantly retail and neighborhood serving focus of the Transition and Northern Districts.

2.4.1 PERMITTED AND CONDITIONALLY PERMITTED USES

Primary uses for each of the three Districts are generally described in Section 2.3, while Table 2-1, below, provides specific direction on permitted (P) and conditionally permitted uses (CUP) for each district. Uses not listed in Table 2-1 are prohibited (refer to the Glossary for definition of uses.)

Conditional Use Permits shall meet the intent of the North 40 Goals, Policies, Vision Statement, and Guiding Principles.

2.4.2 COMMERCIAL USES

Projects proposing new commercial square footage must present the proposal to the Conceptual Development Advisory Committee. The application submittal must include an Economic Impact Study to assess economic competitiveness.

TABLE 2-1 PERMITTED LAND USES				
		LARK DISTRICT	TRANSITION DISTRICT	NORTHERN DISTRICT
COMMERCIAL				
A.	RETAIL	P	P	P
B.	FORMULA RETAIL BUSINESS	--	P	P
C.	MARKET HALL/ SPECIALTY MARKET	--	P	P
D.	ESTABLISHMENT SELLING ALCOHOLIC BEVERAGES FOR CONSUMPTION OFF-PREMISES	CUP	CUP	CUP
E.	ESTABLISHMENT SELLING ALCOHOLIC BEVERAGES FOR CONSUMPTION ON PREMISES			
	• IN CONJUNCTION WITH A RESTAURANT	P	P	P
	• WITHOUT FOOD SERVICE (BAR)	--	CUP	CUP

TABLE 2-1 PERMITTED LAND USES				
		LARK DISTRICT	TRANSITION DISTRICT	NORTHERN DISTRICT
COMMERCIAL (CONTINUED)				
F.	DRIVE-UP WINDOW FOR ANY BUSINESS, EXCLUDING RESTAURANTS	--	--	CUP
G.	SUPER DRUGSTORE	--	--	P
H.	SUPERMARKET	--	--	P
I.	RESTAURANT	P	P	P
J.	PERSONAL SERVICE	P	P	P
K.	OFFICE ¹	P	P	P
L.	HOTEL	--	P	P
M.	FINANCIAL INSTITUTION	P	P	P
RESIDENTIAL				
A.	COTTAGE CLUSTER	CUP	--	--
B.	TOWNHOMES/ GARDEN CLUSTER	P	P	--
C.	ROWHOUSES	P	P	--
D.	MULTI-FAMILY	P	P	p ²
E.	CONDOMINIUMS	P	P	p ²
F.	LIVE/WORK LOFTS	--	P	p ²

Note:

1. Medical Office is only permitted on Assessor Parcel Numbers 424-07-102 through -112, 424-07-099, and 424-06-129.

2. Residential only allowed in Northern District when located above commercial.

TABLE 2-1 PERMITTED LAND USES				
		LARK DISTRICT	TRANSITION DISTRICT	NORTHERN DISTRICT
RECREATION/ENTERTAINMENT				
A.	OUTDOOR ENTERTAINMENT	--	CUP	P
B.	LIVE THEATER	--	--	CUP
C.	HEALTH CLUB	--	CUP	CUP
D.	COMMERCIAL RECREATION AND AMUSEMENT ESTABLISHMENT	--	--	CUP
COMMUNITY SERVICES				
A.	CLUB, LODGE, HALL, FRATERNAL ORGANIZATION	CUP	CUP	CUP
B.	PARK, PLAZA, PLAYGROUND	P	P	P
C.	PUBLIC BUILDING	P	P	P
D.	PUBLIC TRANSPORTATION AND PARKING FACILITIES	P	P	P
E.	RELIGIOUS INSTITUTIONS	CUP	CUP	CUP

TABLE 2-1 PERMITTED LAND USES				
		LARK DISTRICT	TRANSITION DISTRICT	NORTHERN DISTRICT
SCHOOLS				
A.	PRIVATE AND PUBLIC	CUP	CUP	CUP
B.	INSTITUTIONAL SCHOOLS/STUDIOS (ARTS/CRAFTS, MUSIC, YOGA, DANCE, KARATE, ETC.)	CUP	CUP	CUP
C.	DAY CARE CENTER	CUP	CUP	CUP
D.	SMALL FAMILY DAY CARE HOME	P	P	P
E.	LARGE FAMILY DAY CARE HOME	CUP	CUP	CUP
TRANSMISSION FACILITIES/UTILITIES				
A.	TRANSMISSION FACILITIES AND UTILITIES	CUP	CUP	CUP
B.	RADIO/BROADCAST STUDIOS	CUP	CUP	CUP

TABLE 2-1 PERMITTED LAND USES				
		LARK DISTRICT	TRANSITION DISTRICT	NORTHERN DISTRICT
AUTOMOTIVE (VEHICLE SALES, SERVICE AND RELATED ACTIVITIES)				
A.	CAR WASH	--	--	CUP
B.	NEW VEHICLE SALES AND RENTAL	--	--	CUP
C.	ALTERNATING USE/SHARED PARKING OF OFF-STREET PARKING SPACES	P	P	P
AGRICULTURE AND ANIMAL SERVICES				
A.	BOTANICAL NURSERY	P	P	P
B.	VETERINARY HOSPITAL (WITHOUT KENNEL)	--	CUP	CUP
OTHER				
A.	24 BUSINESSES OR BUSINESSES OPEN BETWEEN THE HOURS OF 2:00 AND 6:00 AM	--	--	CUP

2.4.3 NON-CONFORMING USES

Existing legal uses that are not permitted or listed in Table 2-1 are declared non-conforming uses. Refer to Division 5 of Article 1 of the Zoning Ordinance for definitions and regulations for managing non-conforming uses including:

- Continuation of an existing use.
- Repair, alteration, and maintenance.
- Expansion of legal non-conforming uses, buildings, or structures.

2.5 AREA-WIDE DEVELOPMENT REGULATIONS

An important objective of this Specific Plan is to set the parameters for development within the Specific Plan Area. The goal is to offer a compatible mix of land uses in a pedestrian-friendly environment. The following development standards tailor the existing Town standards to shape the desired built form and ensure compatibility with the surrounding neighborhoods and Los Gatos’ small town character. This section initially addresses development regulations that apply throughout the Specific Plan Area, followed by development regulations that are specifically designed for non-residential and residential land uses.

2.5.1 MAXIMUM DEVELOPMENT CAPACITY

A maximum development capacity of 501,000 square feet (sf) has been provided to limit the overall build-out of the Specific Plan Area and provide an appropriate balance of land uses that meet the goals and objectives of the Specific Plan.

Table 2-2 defines maximums of 250,000 sf of new office/hotel, 400,000 sf of other new commercial (includes: restaurants, retail, specialty market, health club, personal services and entertainment), and 270 residential units. More restrictive than the Town’s General Plan, the Specific Plan has a maximum capacity of 501,000 sf which includes 435,000 sf of new non-residential square footage and 66,000 sf of existing commercial uses.

Development Capacity Submittal Requirement:

Every application for Architecture and Site Review shall include a table that identifies the following:

- Proposed project building floor area categorized by land use.
- New total developed building floor area categorized by land use.
- Balance of allowed land use square footage shown in Table 2-2.

TABLE 2-2 MAXIMUM DEVELOPMENT CAPACITY		
LAND USE	UNITS	SQUARE FEET
RESIDENTIAL	270*	REFER TO SECTION 2.7.3
OFFICE/HOTEL		250,000
COMMERCIAL (EXCLUDING OFFICE/ HOTEL) <ul style="list-style-type: none"> • RESTAURANTS • RETAIL • SPECIALTY MARKET • HEALTH CLUB • PERSONAL SERVICE (BEAUTY SUPPLY, NAIL SALON, ETC.) • ENTERTAINMENT 		400,000
NOTE: THE NEW NON-RESIDENTIAL PORTION OF THE PROJECT SHALL INCLUDE A MIXTURE OF COMMERCIAL (SHOPPING CENTER), AND/OR HOTEL, AND/OR STAND-ALONE GENERAL OFFICE THAT DOES NOT CREATE A SIGNIFICANT UNAVOIDABLE IMPACT AS A RESULT OF THE DEVELOPMENT. THE TOTAL NEW SQUARE FOOTAGE SHALL NOT EXCEED 435,000 SQUARE FEET (SF). WITH THE EXCEPTION OF ASSESSOR PARCEL NUMBERS 424-07-102 THROUGH -112, 424-07-099, AND 424-06-129, NO NEW MEDICAL OFFICE WILL BE PERMITTED. IF DESTROYED, THE EXISTING BUILDINGS ON THE PARCELS REFERENCED ABOVE ARE ALLOWED TO REBUILD IN SUBSTANTIALLY THE SAME MANNER AS THEY EXISTED BEFORE THEIR DESTRUCTION. THE EXISTING 66,000 SF OF RECENTLY CONSTRUCTED BUILDINGS ON THE PARCELS REFERENCED ABOVE IS IN ADDITION TO THE 435,000 SF OF NEW NON-RESIDENTIAL SQUARE FOOTAGE. EACH PROJECT SHALL PROVIDE A CURRENT TRAFFIC ANALYSIS DEMONSTRATING COMPLIANCE WITH THIS REQUIREMENT. PROJECTS CANNOT EXCEED THE MAXIMUM TRAFFIC CAPACITY EVALUATED IN THE EIR *TOTAL NUMBER OF UNITS, INCLUDES EXISTING UNITS AND TOWN REQUIRED BELOW MARKET PRICE UNITS. ACTION HOU-1.3 GENERAL PLAN DENSITY BONUS DOES NOT APPLY TO THE SPECIFIC PLAN AREA.		

2.5.2 BUILDING HEIGHT

- a. The maximum height of any building, excluding affordable housing and hotel uses, is 35 feet with the following criteria:
 - i. Maximum building height shall be determined by the plumb vertical distance from the natural or finished grade, whichever is lower and creates a lower profile, to the uppermost point of the roof edge, wall, parapet, mansard, or other point directly above that grade. For portions of a structure located directly above a cellar, the height measurement for that portion of the structure shall be measured as the plumb vertical distance from the existing natural grade to the uppermost point of the structure directly over that point in the existing natural grade. No point of the roof or other structural element within the exterior perimeter of the structure shall extend beyond the plane established by the maximum height plane. Maximum building height includes all elements and height exceptions are not permitted within the Specific Plan Area.
 - ii. Lark District - 15% of the overall development provided (building footprint) within the Lark District shall be structures of a maximum of two-stories with a 25 foot maximum height. The majority of this requirement may be provided within the Perimeter Overlay Zone (refer to Section 2.5.7). Every application for Architecture and Site Review shall include a table that identifies the following:
 - Total building footprint square footage within the Lark District existing at the time of the application submittal.
 - Percent of total building footprint square footage located within the Lark District currently satisfying the 15% height requirement at the time of submittal.

- New total percentage of building footprint square footage located within the Lark District that satisfies the 15% requirement (for height) following application submittal.
- b. The maximum height is 45 feet for a hotel and/or a mixed-use and/or mixed-income development including a minimum of 40% extremely low, very low, or low income affordable housing units.

2.5.3 OPEN SPACE GOALS AND POLICIES

The Specific Plan Area shall encourage outdoor activity by integrating a variety of open spaces such as pocket parks, parks and plazas, common gathering areas, courtyards, pedestrian paseos, clubhouse and barbecue areas, walkable streets lined with large shade trees and active streetscape, landscaped buffers, and ample sitting areas. This neighborhood will be designed to serve the unmet needs of Los Gatos, providing an environment where people live and walk or bicycle to a nearby coffee shop, wine bar, and restaurants.

Goal: *To integrate an interconnected system of open spaces, parks and plazas within the Specific Plan Area.*

Open Space Policies:

Policy O1: View Preservation

Promote and protect views of hillsides and scenic resources.

Policy O2: Landscape Buffer

Establish a landscaped buffer along the North 40 perimeter.

Policy O3: Neighborhood Open Space Network

Provide an open space network of neighborhood parks, passive open space, plazas, pedestrian paseos, landscape buffers and/or common open space per Specific Plan Open Space Standards.

Policy O4: Common Space

Provide a space for small gatherings and social activities that minimize conflicts with adjacent uses and competition with Downtown.

Policy O5: Pedestrian Amenities

Provide adequate pedestrian amenities such as street trees, benches, pedestrian-level street lighting and other street furnishings.

Policy O6: Orchard Planting

Integrate orchard style planting within the Specific Plan Area as a gesture towards the site's historic agricultural use.

2.5.4 OPEN SPACE STANDARDS

To ensure that adequate open space is integrated into future development in the Specific Plan Area, a minimum of 30% of open space is required (Table 2-3). This 30% requirement should be a variety of green-spaces and plaza spaces dispersed throughout the different districts. By specifying minimum open space requirements/standards, the Specific Plan provides incentives for the consolidation of parking into podium parking and parking structures, minimizing at-grade parking, minimizing road widths, and increasing pedestrian spaces.

a. Open space means a ground plane open and generally unobstructed from the ground plane to the sky. Balconies, shade structures, and roof eaves may extend over a portion of the open space. Open space includes both “green open space”

and “hardscape” (plazas, courtyards, pathways, sidewalks, and pedestrian paseos). Plazas, courtyards, and planters over podium parking or on roof decks also qualify as open space.

- b. To ensure the open space is distributed throughout the Specific Plan Area, a minimum of 30% open space shall be provided across the entire Specific Plan Area. The 30% requirement shall be calculated for each application or group of applications.
- c. The 30% open space requirement shall include a variety of green and plaza spaces with a minimum of 20% being green space.
 - i. Green Space/Green Open Space: for purposes of this Specific Plan and calculating open space requirements green space and green open space is grass or landscaped areas. These can include but are not limited to parks, bioretention, common and private residential green space, planters larger than 50 square feet, landscaped planting strips, drivable turf-block, and parking lot landscaping. Trees planted in tree wells shall not be calculated as part of the green space requirement.
 - ii. Hardscape: for purposes of this Specific Plan and calculating open space requirements, hardscape refers to private or common paved areas for the use of pedestrians including plazas, courtyards, pathways, sidewalks, and pedestrian paseos. Roads and parking areas shall not be calculated as part of the open space or hardscape requirement.
- d. 20% of the 30% open space requirement shall be publicly accessible.

- e. Every application for Architecture and Site Review shall include an exhibit(s) that shows the open space and pedestrian network.
- f. Remodels of existing structures along Los Gatos Boulevard that do not change more than 50% of the existing footprint are exempt from the 30% open space requirement.

The following table provides the requirements for open space in the Specific Plan Area:

TABLE 2-3 MINIMUM OPEN SPACE REQUIREMENTS	
OPEN SPACE DESIGNATION (EXCLUDING PARKING AND ROADWAYS)	PERCENT OF SPECIFIC PLAN AREA
GREEN OPEN SPACE	20% MINIMUM
HARDSCAPE (PLAZAS/ COURTYARDS/PATHWAYS/ SIDEWALKS AND PEDESTRIAN PASEOS) AND/OR ADDITIONAL GREEN OPEN SPACE	REMAINDER OF REQUIRED OPEN SPACE
TOTAL OPEN SPACE	30% MINIMUM

2.5.5 TYPES OF OPEN SPACE

a. Plazas/Paseos/Pathways

Plazas, paseos, and pathways create vibrant pedestrian-oriented spaces linking the residential and commercial neighborhoods with adjacent uses, parks, and streets. Throughout the Specific Plan Area, plazas, paseos, and pathways will be incorporated to accommodate different types of activities. These neighborhood gathering spaces shall serve to establish a sense of place and identity.

In the Transition and Northern Districts, plazas, paseos, and pathways can provide space for private outdoor dining, events, and street side entertainment. They should be well-designed, providing ongoing opportunities for human activities that create an interactive environment, build a sense of community, and create opportunities for social interaction. Paseos shall be provided to link public parking and the street environment and residential projects with adjacent streets and plazas/parks. Amenities in plazas and paseos may include:

- Flexible spaces for outdoor dining and gathering
- Well-lit spaces, pedestrian oriented lighting, bollards, and wall-mounted lights
- Articulated edges that create interesting nodes and gathering spaces
- Sitting areas
- Water feature(s)
- Landscaping
- Safe and convenient connections to adjacent uses

- Public art
- Special paving: decorative pavers, decomposed granite, cobblestone
- Focal points such as architectural structures, sculptures, and fountains
- Multi-modal linkages

In the Lark District the pathways will provide connections between common areas and pocket parks and links to the streets and the neighborhood commercial areas in the Transition District. These pathways should provide safe and convenient access as well as an amenity to the neighborhood.

Pathways might include:

- Paved or permeable surfaces
 - Landscaping
 - Orchard planting
 - Sitting areas
 - Pedestrian oriented lighting
 - Public art
 - Focal points such as gardens, trellises, or art
- b. Perimeter Buffer/Perimeter Overlay Zone (refer to Section 2.5.6)
- The perimeter of the site is intended to emphasize the character and heritage of Los Gatos. Large trees and shrubs will surround the Specific Plan Area on the north and west sides (refer to Landscape Palette in Chapter 3), creating a

natural buffer between future development and Highways 17 and 85. The buffer should also provide an opportunity to incorporate walking paths and sitting areas for passive recreation.

- Along Los Gatos Boulevard and Lark Avenue, orchard planting will be used in the buffer to reflect the agricultural heritage of the site.

c. Common Recreational Amenities

A variety of recreational amenities shall be incorporated as residential common areas. These areas may be either common areas for the residents of a particular building and/or open to the public. They are intended to provide a variety of opportunities to gather with friends and family.

d. Orchard Planting

Orchards are an integral part of the Specific Plan Area and reflect the historic character of Los Gatos. Both Los Gatos Boulevard and Lark Avenue shall contain a landscape buffer planted with orchard trees identified within the Landscape Palette provided in Chapter 3. Streets, parking lots, pocket parks, plazas, and paseos should also be considered as an opportunity to integrate an orchard feel.

2.5.6 DISTRICT EDGE OVERLAY ZONE

All uses permitted within 100 feet of a District boundary line shall be permitted within the District Edge Overlay.

2.5.7 PERIMETER OVERLAY ZONE

The following standards apply within the Perimeter Overlay Zone:

- a. Buildings or portions of buildings located within 50 feet of Lark Avenue shall be restricted to a maximum building height of 25 feet.
- b. Buildings or portions of buildings located within 50 feet of Los Gatos Boulevard shall be restricted to a maximum building height of 25 feet.
- c. Additional setback requirements are provided in Table 2-5 of this chapter.
- d. No building shall be located within 30 feet of a property line adjacent to the freeway.

2.5.8 PARKING REQUIREMENTS

Parking provided within the Specific Plan Area shall adhere to the standards provided in Division 4 of the Zoning Ordinance.

Number of Off-Street Spaces Required:

- a. Non-Residential Use: The number of off-street parking spaces shall be consistent with parking required in Downtown as required within Division 4 of the Zoning Ordinance.
- b. Residential Use: Parking provided within the Specific Plan Area shall be as follows:

TABLE 2-4 RESIDENTIAL OFF-STREET PARKING SPACE REQUIREMENT	
UNIT TYPE	SPACES REQUIRED
SENIOR/AFFORDABLE HOUSING UNIT	.5 SPACE + .5 GUEST
<i>NOTE: DIRECTOR OF COMMUNITY DEVELOPMENT SHALL APPROVE THIS REQUIREMENT GIVEN THAT THE APPLICANT PRESENTS FINDINGS THAT THE PRODUCT TYPE PARKING DEMAND WARRANTS .5 SPACES PER UNIT AND SURROUNDING NEIGHBORHOODS WILL NOT BE NEGATIVELY IMPACTED.</i>	
1 BEDROOM UNIT	1 SPACE + .5 GUEST
2+ BEDROOM UNIT	2 SPACES + .5 GUEST

Mixed-Use Parking:

When a project contains a vertical mix of uses composed of commercial uses with residential, hotel, and/or office use above in the same building, the non-residential portion of the mixed-use building may be eligible to receive a reduction in the parking requirements established by this Specific Plan of up to 25%, subject to Architecture and Site Review and approval.

Shared Parking Reduction:

Shared (alternating use) parking agreements may be developed if two or more land uses or businesses have different hours of operation or hours that do not substantially overlap with each other (e.g., office).

When the above criteria are met, such uses may develop shared parking agreements to satisfy the parking requirements of this Specific Plan, with review and approval by Director of Community Development and in accordance with the following:

- a. Only 50% of the required parking may qualify for the shared arrangement.
- b. A minimum of 50% of the required parking must be met on-site, notwithstanding the parking reduction provisions of this Specific Plan.
- c. Required parking must be calculated based on the land use that demands the largest amount of parking.
- d. Dedicated parking spaces shall be provided for residents and shall be clearly distinguished from spaces provided for commercial and/or office uses. Residential guest parking, commercial, and office parking may be shared.

Tandem Parking:

- a. Non-Residential Use: Up to 40% of the required on-site non-residential parking may be provided as tandem parking with an attendant upon approval by the Director of Community Development.
- b. Residential Use: Residential units may use tandem parking. Tandem spaces shall be designated by unit so a single owner/tenant has control of all spaces associated with their unit. A provision of shuffle stalls (extra stall to allow for temporary parking) shall be considered in design.

Parking Structures:

- a. Maximum height of a parking structure shall not exceed maximum building height requirements and shall be measured from the adjacent street grade, without restrictions on the number of internal stories.

- b. Setbacks shall be heavily landscaped in accordance with the Landscape Palette provided in Chapter 3.
- c. Parking structures fronting the Neighborhood Street shall be wrapped with commercial space at the ground floor.
- d. Parking structure facades visible from Primary Streets over 150 feet in length shall incorporate at least one or more of the following:
 - Differentiation of the ground floor from upper floors.
 - Changes in architectural materials.
 - Projecting forward or recessing back portions or elements of the parking structure facade.
 - Horizontal openings broken up with vertical columns to create a rhythm of openings similar to a building with windows.

2.5.9 PRIMARY STREET FRONTAGE SETBACKS

Primary Streets are identified as Lark Avenue, Los Gatos Boulevard, South 'A' Street, North 'A' Street, Neighborhood Street, Noddin Avenue, Burton Road, and Commercial Streets within the Northern District. Interior residential streets or lanes within the Lark and Transition District have setbacks outlined in Section 2.7.5.

- a. Los Gatos Boulevard and Lark Avenue setbacks shall be measured from future right-of-way (back of future right-of-way dedication) and shall contain orchard planting as defined by Section 2.5.2 - Open Space Goals and Policies.
- b. All internal setbacks are measured from the face of curb.

- c. Setback diagrams containing an asterisk (*) are permitted a five foot encroachment for up to 50% of linear building facade length to allow for porches, balconies, and other building elements (including livable space) that will add visual interest and minimize the appearance of a solid wall plane.
- d. Each Primary Street within a district will have a designated street tree type (refer to Landscape Palette in Chapter 3) and trees shall be spaced to establish a rhythm.
- e. All landscaped areas and planting strips shall be planted with trees identified in the Landscape Palette provided in Chapter 3 of this document.
- f. Parking is not permitted within frontage setbacks with the exception of Los Gatos Boulevard. Parking on Los Gatos Boulevard may encroach up to five feet into the 30 foot setback for no more than a 100 foot span.
- g. Buildings within the Transition and Northern Districts shall be oriented toward the street with parking lots provided in the rear and sides to create a walkable neighborhood. When buildings do setback from the sidewalk, allowable uses in the front of the building include plazas or pedestrian spaces, sidewalks, landscaping, retail display areas and café or outdoor seating areas.
- h. Minimum street frontage setbacks on Primary Streets in the North 40 Specific Plan Area shall be as follows in Table 2-5: (Refer to Table 2-6 for non-residential side and rear setbacks, and Section 2.7.5 for interior residential street setbacks in the Lark District. Refer to Chapter 4, Circulation and Streetscape to view street sections, illustrated from building face to building face.)

TABLE 2-5 PRIMARY FRONTAGE SETBACKS

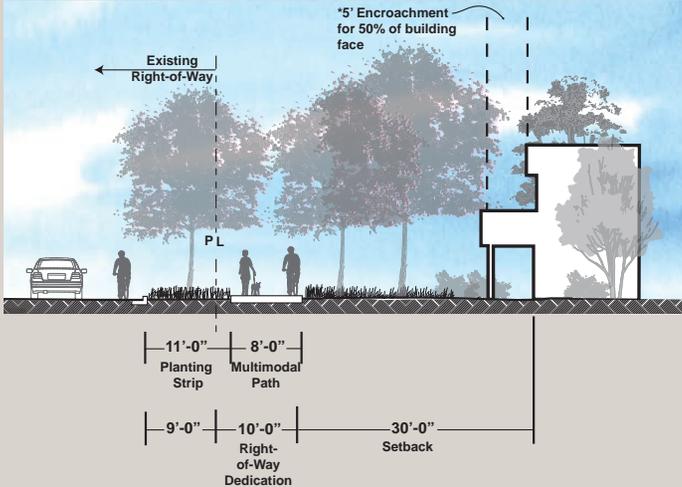
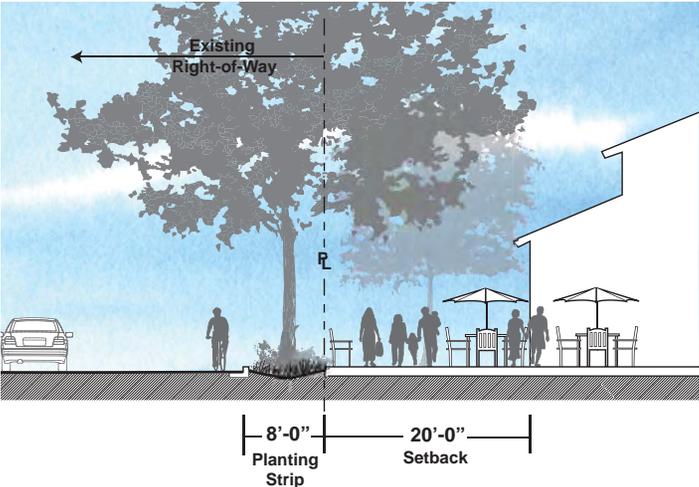
	STREET	SETBACK	SETBACK DESIGN	ILLUSTRATIVE - CONCEPTUAL SETBACK EXAMPLES
1.	LARK AVENUE	<p>MINIMUM 30' PLUS 10' RIGHT-OF-WAY DEDICATION</p> <p>NOTE: 5' ENCROACHMENT PERMITTED FOR UP TO 50% OF LINEAR BUILDING FACADE LENGTH</p>	<p>10' RIGHT-OF-WAY DEDICATION PLUS 30' SETBACK</p> <p>SETBACK DESIGN SHALL INCLUDE LANDSCAPED AREA PLANTED WITH ORCHARD TREES AND A MULTIMODAL PATH</p> <p>NEW STREET AND/OR ORCHARD TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p>	 <p>The diagram illustrates a street cross-section with an 'Existing Right-of-Way' line. From the street edge, there is an 11'-0" planting strip containing trees and a 9'-0" right-of-way dedication. This is followed by an 8'-0" multimodal path. A 30'-0" setback is shown from the street edge to the building facade. A 5' encroachment is permitted for 50% of the building facade. A 'PL' (Planting Line) is marked at the start of the planting strip.</p>
2.	LOS GATOS BOULEVARD IF MARKET HALL IS CONSTRUCTED AT EDGE	<p>MINIMUM 20'</p>	<p>EXISTING RIGHT-OF-WAY</p> <ul style="list-style-type: none"> • SIDEWALK WITH TREES PLANTED IN TREE GRATES • OR A PLANTING STRIP • OR RAISED PLANTERS <p>SETBACK DESIGN SHALL INCLUDE A 20' AREA TO BE HARDSCAPED AND DESIGNED AS A PLAZA WITH TREES PLANTED IN TREE GRATES.</p> <p>NEW STREET AND/OR ORCHARD TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p>	 <p>The diagram shows a street cross-section with an 'Existing Right-of-Way' line. An 8'-0" planting strip with trees is shown. A 20'-0" setback is indicated from the street edge to the building facade. A 'PL' (Planting Line) is marked at the start of the planting strip.</p>

TABLE 2-5 PRIMARY FRONTAGE SETBACKS

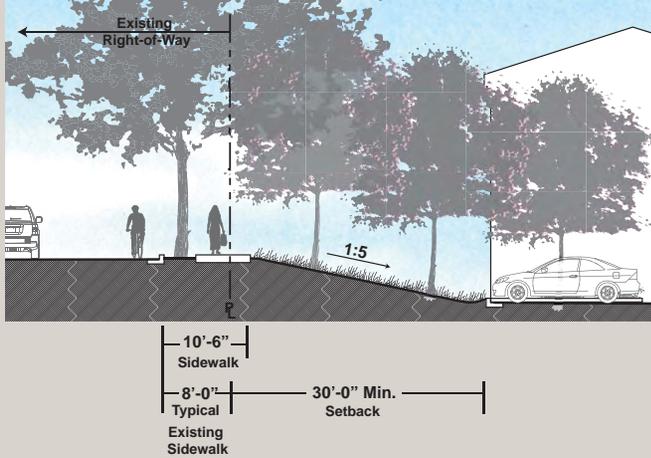
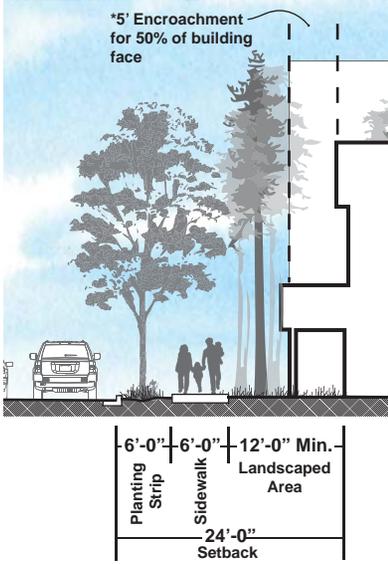
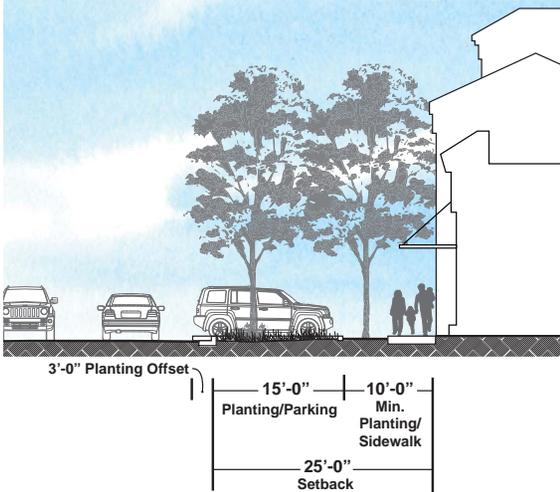
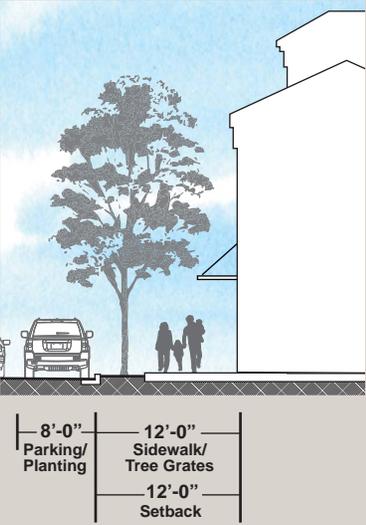
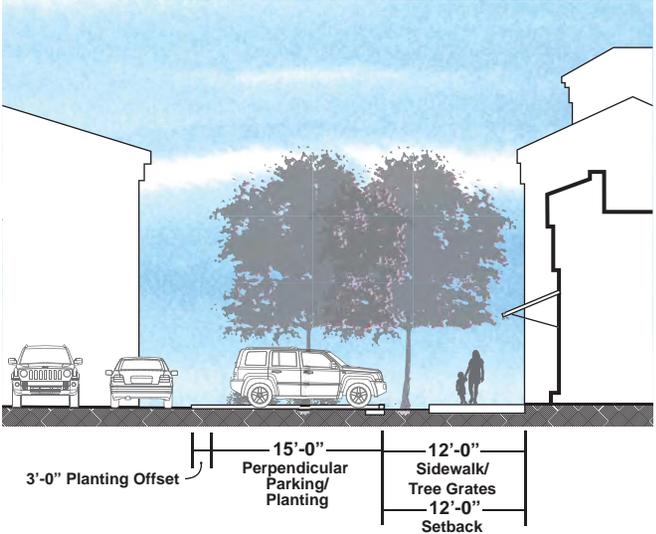
STREET	SETBACK	SETBACK DESIGN	ILLUSTRATIVE - CONCEPTUAL SETBACK EXAMPLES
NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.			
3.	<p>LOS GATOS BOULEVARD IN ALL OTHER AREAS</p>	<p>MINIMUM 30'</p> <p>NOTE: PARKING MAY ENCROACH UP TO 5' INTO THE 30' SETBACK FOR NO MORE THAN A 100' SPAN.</p>	<p>EXISTING RIGHT-OF-WAY</p> <ul style="list-style-type: none"> SIDEWALK WITH TREES PLANTED IN TREE GRATES OR A PLANTING STRIP <p>SETBACK DESIGN SHALL INCLUDE A 30' LANDSCAPED AREA PLANTED WITH ORCHARD TREES.</p> <p>NEW STREET AND/OR ORCHARD TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p> 
4.	<p>SOUTH 'A' STREET - LARK DISTRICT</p>	<p>MINIMUM 24'</p> <p>NOTE: 5' ENCROACHMENT PERMITTED FOR UP TO 50% OF LINEAR BUILDING FACADE LENGTH</p>	<p>SETBACK DESIGN SHALL INCLUDE 6' PLANTING STRIP CONTAINING STREET TREES, 6' SIDEWALK AND 12' MINIMUM LANDSCAPED AREA</p> <p>TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p> <p>GARAGE DOORS SHALL NOT DIRECTLY FACE 'A' STREET</p> <p>*5' Encroachment for 50% of building face</p> 
NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.			

TABLE 2-5 PRIMARY FRONTAGE SETBACKS

	STREET	SETBACK	SETBACK DESIGN	ILLUSTRATIVE - CONCEPTUAL SETBACK EXAMPLES
5.	NEIGHBORHOOD STREET	MINIMUM 15'	<p>SETBACK DESIGN SHALL INCLUDE 15' AREA TO BE HARDCAPED AND DESIGNED AS A PLAZA WITH A MINIMUM OF 8' CLEARANCE AT BUILDING EDGE, 5' WIDE LANDSCAPED PLANTERS/TREE GRATES CONTAINING STREET TREES</p> <p>TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p>	 <p>The diagram shows a cross-section of a street with a building on the right. A tree is planted in a planter area that is 15 feet wide from the building edge. The tree's canopy extends over the sidewalk, providing a minimum clearance of 8 feet from the building edge to the top of the canopy. A car and two people are shown on the sidewalk for scale.</p>
6.	SOUTH 'A' STREET - TRANSITION DISTRICT	MINIMUM 25'	<p>SETBACK DESIGN SHALL INCLUDE 10' AREA TO BE HARDCAPED WITH STREET TREES IN 5' WIDE TREE GRATES</p> <p>TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p> <p>GARAGE DOORS SHALL NOT DIRECTLY FACE 'A' STREET</p> <p>POCKETS OF DIAGONAL, PERPENDICULAR, OR PARALLEL PARKING ARE ALLOWED BETWEEN PLANTERS. REFER TO CHAPTER 4, CIRCULATION AND STREETScape FOR DIMENSIONS</p>	 <p>The diagram shows a cross-section of a street with a building on the right. A 3-foot planting offset is shown from the building edge to the start of the planting area. Within this area, there is a 15-foot planting/parking zone and a 10-foot minimum planting/sidewalk zone. The total setback from the building edge is 25 feet. A car and two people are shown for scale.</p>

NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.

TABLE 2-5 PRIMARY FRONTAGE SETBACKS

	STREET	SETBACK	SETBACK DESIGN	ILLUSTRATIVE - CONCEPTUAL SETBACK EXAMPLES
7.	NORTH 'A' STREET, NODDIN AVENUE, AND BURTON ROAD	MINIMUM 12'	<p>SETBACK DESIGN SHALL INCLUDE 12' AREA TO BE HARDSCAPED OR DESIGNED AS A PLAZA WITH STREET TREES IN 5' WIDE TREE GRATES</p> <p>TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p> <p>GARAGE DOORS SHALL NOT DIRECTLY FACE 'A' STREET</p>	 <p>8'-0" Parking/Planting</p> <p>12'-0" Sidewalk/Tree Grates</p> <p>12'-0" Setback</p>
8.	COMMERCIAL STREETS WITHIN NORTHERN DISTRICT (C2 STREET)	MINIMUM 12'	<p>SETBACK DESIGN SHALL INCLUDE 12' AREA TO BE HARDSCAPED OR DESIGNED AS A PLAZA WITH STREET TREES IN TREE GRATES</p> <p>TREES SHALL BE SELECTED FROM THE LANDSCAPE PALETTE LOCATED IN CHAPTER 3 OF THIS SPECIFIC PLAN</p> <p>POCKETS OF DIAGONAL, PERPENDICULAR, OR PARALLEL PARKING ARE ALLOWED BETWEEN PLANTERS. REFER TO CHAPTER 4, CIRCULATION AND STREETScape FOR DIMENSIONS</p>	 <p>3'-0" Planting Offset</p> <p>15'-0" Perpendicular Parking/Planting</p> <p>12'-0" Sidewalk/Tree Grates</p> <p>12'-0" Setback</p>

NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.

2.5.10 MIXED-USE PROJECTS

Intent

Mixed-use projects combine retail, office and/or residential uses into one single development. The uses can be combined within the same building or in separate buildings on the site. Both types of mixed-use development are encouraged.

Design Criteria

Mixed-use projects can create unique design issues, such as the need to balance the requirements of residential uses with the needs of commercial uses. When designing mixed-use developments, it is important that commercial and office uses are sensitive to the residential uses of the project and adhere to the following requirements:

- a. Commercial portions of a mixed-use project shall adhere to standards pertaining to commercial uses within this Specific Plan.
- b. Residential portions of a mixed-use project shall adhere to standards pertaining to residential development within this Specific Plan.
- c. In the Northern District, residential uses are not permitted on the ground floor; however, a central lobby may be located on the ground floor.
- d. Dedicated parking spaces shall be provided for residents and shall be clearly distinguished from spaces provided for commercial and/or office uses. Residential guest parking, commercial and office parking may be shared. Residential parking must be provided on-site.
- e. Buildings shall be designed and sited to reduce odor, noise, light and glare, as well as visual or other conflicts between commercial and residential uses.
- f. Lighting for the commercial uses shall be appropriately shielded so as not to spill over into the residential area or negatively impact the residential units.
- g. Loading areas and trash and recycling enclosures for commercial uses shall be located away from residential units when feasible and screened properly.
- h. Commercial uses with residential units either above or attached shall provide ventilation systems to prevent odors from adversely affecting residential units.

2.5.11 SIGNAGE REQUIREMENTS

General Requirements

Signage shall conform to the guidelines established in Section 3.5, Signage, of Chapter 3 within this document.

- a. Each application shall provide a comprehensive sign program to define the neighborhood identity as described in Section 3.4, Neighborhood Identity, of Chapter 3 within this document.
- b. The quantity and location of North 40 Neighborhood identification signage (entry features) is subject to determination by the Architecture and Site Review deciding body.
- c. Neighborhood identity features are intended to encourage creativity, artful and illustrative design features. They may contain graphics and Neighborhood/Project Name and shall not be defined as a sign within the Specific Plan Area.

- d. Neighborhood identity features and ground signs shall appear to be made of stone, steel, corten steel, wood, and/or other natural materials.

Total sign area per user is to be based on a portion of that user's total sign area allowance based on one square foot of sign per linear foot of user's "primary" building frontage.

- e. Signs are prohibited if the dominant purpose is to be seen by freeway travelers. Signs on buildings that are incidentally visible are permitted depending on the sign size. For the purpose of evaluating appropriate sign size, the Town will consider the normal sign viewing distances, the general nature of the street or walkway (e.g. width and traffic speed) and the size of the signs in the area.
- f. Signage requirements provided within the Specific Plan Area shall adhere to the standards provided in Division 3 of the Zoning Ordinance with the following exceptions.

Exceptions

- a. Roof signs are permitted on the Market Hall if designed as an architectural element or reflective of historical market oriented signage.
- b. The Architecture and Site Review deciding body may make exceptions to the standards and guidelines contained within this document, when the applicant demonstrates that such exceptions are necessary or appropriate to the proposed use or architectural character.

2.6 NON-RESIDENTIAL/MIXED-USE DEVELOPMENT STANDARDS

2.6.1 INTENT

The following development standards supplement the previous Area-wide Standards and apply to any development that is not a 100% residential use.

2.6.2 AREA

There is no minimum lot area requirement for a non-residential use.

2.6.3 LOT AREA COVERAGE

- a. Maximum building coverage of a lot designated for a non-residential use or mixed-use, including any type of accessory building, is 50%. This requirement shall be calculated for each application or group of applications.

2.6.4 SETBACKS (NON-RESIDENTIAL INTERIOR TO SITE)

- a. All setbacks interior to site are measured from the face of curb. Lark Avenue and Los Gatos Boulevard are measured from property line.
- b. All landscaped areas and planting strips shall be planted with trees identified within the Landscape Palette provided in Chapter 3 of this document.
- c. Minimum front, side, and rear setbacks for non-residential/mixed-use buildings on interior streets in the Transition and Northern Districts shall be as follows (see Table 2-5 for Primary Street Setbacks):

TABLE 2-6 NON RESIDENTIAL/MIXED-USE SETBACKS

1.	<p>BUILDINGS SHALL BE ORIENTED TOWARD THE STREET WITH PARKING IN THE REAR TO CREATE A VIBRANT AND WALKABLE NEIGHBORHOOD. WHEN BUILDINGS DO SET BACK FROM THE SIDEWALK, ALLOWABLE USES IN THE FRONT OF THE BUILDING INCLUDE PLAZAS OR PEDESTRIAN SPACES, SIDEWALKS, LANDSCAPING, RETAIL DISPLAY AREAS, AND CAFÉ OR OUTDOOR SEATING AREAS.</p> <p>THE DECIDING BODY MAY ALLOW A REDUCTION IN THE SETBACK IF IT MAKES ALL OF THE FOLLOWING FINDINGS:</p> <p>A. THE SETBACK PROVIDES FOR ADEQUATE PEDESTRIAN CIRCULATION.</p> <p>B. THE SETBACK IS COMPATIBLE WITH THE ADJACENT BUILDINGS AND COMPLEMENTS THE BUILDINGS IN THE IMMEDIATE VICINITY.</p> <p>C. THE SETBACK PROVIDES ADEQUATE CLEAR SIGHT VISION FOR VEHICULAR TRAFFIC.</p>	12 FEET
2.	SIDE	NONE REQUIRED
3.	REAR	NONE REQUIRED
<p>NOTE: IN THE LARK DISTRICT - WHERE NON-RESIDENTIAL DEVELOPMENT IS LOCATED DIRECTLY ADJACENT TO A RESIDENTIAL USE, A 20-FOOT SETBACK IS REQUIRED FOR THE NON-RESIDENTIAL BUILDING. THIS IS MEASURED FROM THE PROPERTY LINE TO BUILDING FACE.</p>		

2.6.5 PROJECTIONS ALLOWED INTO SETBACKS

- a. A porte cochere may be permitted over a driveway in a required setback subject to Architecture and Site Review approval, provided that it is not more than one story high and is entirely open on at

least three sides, except for the necessary supporting columns and customary architectural features.

- b. Cornices, belt courses, sills, canopies, or other similar architectural features may extend or project into required setbacks not more than 24 inches. Eaves may encroach up to 36 inches.

2.6.6 RETAIL TENANT SPACE SIZE

The Specific Plan allows for a mix of retail sizes, including smaller neighborhood serving stores that will support the new residential, as well as, larger space for commercial uses, such as sit-down restaurants, specialty market, entertainment, and formula retail. The commercial is to serve the unmet needs for the new and surrounding businesses and residential neighborhoods, and be subject to the following conditions:

- a. Floor Area Calculations: Every application for Architecture and Site Review shall include a table that identifies the sizes of each retail space proposed.
- b. The maximum individual commercial retail tenant size is 50,000 square feet.

2.6.7 ACTIVITIES TO BE CONDUCTED INDOORS

All business, services and processes shall be conducted entirely within completely enclosed buildings, except as provided in Article 5 of the Zoning Ordinance and except for outdoor activities specifically authorized by a Conditional Use Permit or approved by a Special Event permit.

2.6.8 FENCING

Fencing is not permitted in required front setbacks or required setbacks abutting a street in any non-residential use, except walls or fences not over three feet high may be erected to screen on-site parking spaces from the street or to separate outdoor dining areas from the sidewalk.

2.6.9 OUTDOOR STORAGE

When a Conditional Use Permit is granted for outdoor storage, the area for storage must be suitably screened from adjoining property by a wall, dense evergreen hedge of trees or other screening planting, or by a solid fence not less than six feet high. Materials shall not be stored in such a manner as to project above the wall, planting or fence.

2.6.10 RECYCLING COLLECTION FACILITIES AND VENDING MACHINES.

Small recycling collection facilities and vending machines may be permitted outdoors subject to the approval of the Director of Community Development and shall be subject to the following conditions:

- a. The facilities cannot be located in any manner so as to decrease the number of required on-site parking spaces.
- b. Vending machines and reverse vending machines are to be located indoors whenever possible and not more than two of these machines are permitted outdoors unless grouped together within a common enclosure.
- c. A trash receptacle is to be located within five feet of any recycling facility.

- d. The machines and facilities must be maintained on a scheduled basis so as to ensure their general upkeep and cleanliness.
- e. If a facility is proposed for a vacant lot, the lot must provide proper traffic circulation consisting of an all-weather surface, including one on-site employee parking space and an adequate drop-off area.
- f. The facility must provide for pedestrian circulation.

2.7 RESIDENTIAL DEVELOPMENT STANDARDS

2.7.1 INTENT

The following development standards supplement the previous Area-wide Standards and apply to any development that is a 100% residential use.

2.7.2 RESIDENTIAL OPEN SPACE

Common Open Space - Consistent with the Zoning Ordinance, 100 square feet/unit of Common Open Space is required for residential condominiums. 200 square feet/unit of Common Open Space is required for multi-family residential other than condominiums.

2.7.3 RESIDENTIAL UNITS

The Specific Plan Area should accommodate a mix of residential product types and sizes to create the character of an authentic neighborhood rather than a typical development project. The following standards set parameters to guide future residential development. Also refer to the Residential Design Guidelines in Chapter 3 of this Specific Plan.

- a. Residential units shall range in size. Refer to Residential Unit Size Mix in Glossary (Chapter 6).
- b. There shall be a maximum of 270 residential units. This is a maximum, not a goal, and includes the affordable housing units required and the existing units.
- c. Affordable housing (Below Market Price housing) requirements shall be met pursuant to Town Code.
- d. New residential shall be a maximum of:
 - 400,000 gross square feet for Cottage Cluster, Garden Cluster, Townhome and Rowhouse products
 - 300,000 net square feet for Condominium, Multi-Family, Apartments and Affordable products
 - These are maximums, not a goal
- e. Single family detached units shall be a maximum of 1,200 square feet and be designed as a cottage cluster product type as defined in Glossary (Chapter 6).

2.7.4 SETBACKS (RESIDENTIAL)

- a. All setbacks interior to the Specific Plan Area are measured from the face of curb. Lark Avenue and Los Gatos Boulevard setbacks are measured from property line.
- b. Residential setbacks along primary streets (Lark Avenue, Los Gatos Boulevard, South 'A' Street, North 'A' Street, Neighborhood Street, Noddin Avenue, and Burton Road) follow setback standards on Table 2-5. Residential setbacks on all other interior

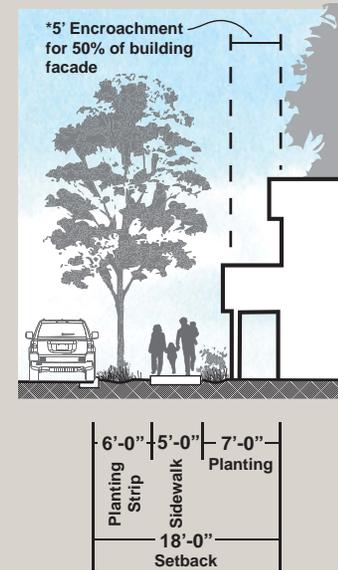
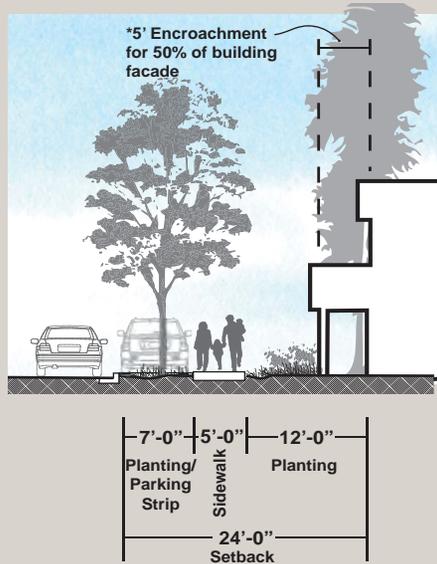
streets shall be determined at Architecture and Site Review. Setback examples are illustrated in Table 2-7, 2-8 and 2-9 to show desired character.

- c. Setback diagrams containing an asterisk (*) are permitted a five foot encroachment for up to 50% of linear building facade length to allow for porches, balconies, and other building elements (including livable space) that will add visual interest and minimize the appearance of a solid wall plane.
- d. All landscaped areas and planting strips shall be planted with trees identified within the Landscape Palette provided in Chapter 3 of this document.
- e. Cornices, belt courses, sills, canopies, cantilevered bay windows, chimneys or other similar architectural features may extend or project into a required setback not more than 30 inches. Eaves may encroach up to 36 inches.

**TABLE 2-7 COTTAGE CLUSTER (DETACHED PRODUCT)/GARDEN CLUSTER (ATTACHED PRODUCT)
LARK DISTRICT**



EXAMPLES



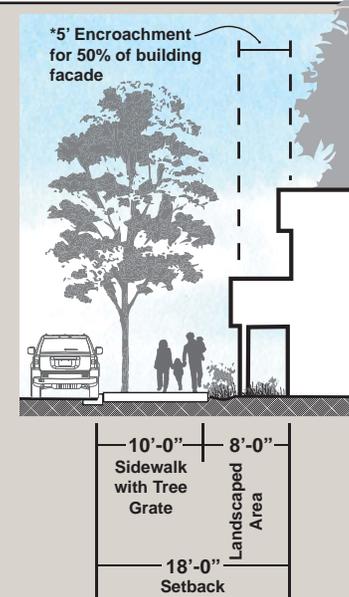
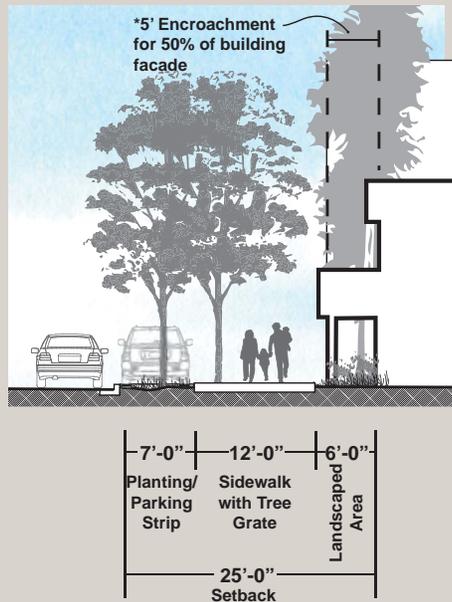
INTERIOR RESIDENTIAL STREET (R2 STREET) - CONCEPTUAL SETBACK EXAMPLES

NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.

**TABLE 2-8 TOWNHOMES AND ROWHOUSES (ATTACHED PRODUCT)
LARK DISTRICT/TRANSITION DISTRICT**



EXAMPLES



INTERIOR RESIDENTIAL STREETS - CONCEPTUAL SETBACK EXAMPLES

NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.

**TABLE 2-9 MULTI-FAMILY, APARTMENTS, AFFORDABLE, MIXED-USE
TRANSITION DISTRICT/NORTHERN DISTRICT**



EXAMPLES



7'-0" - 15'-0"	10'-0"
Planting/Parking	Min. Planting/Sidewalk
17'-0" - 25'-0"	
Setback	



8'-0"	12'-0"
Parking/Planting	Sidewalk/Tree Grates
	12'-0"
	Setback

INTERIOR RESIDENTIAL STREETS - CONCEPTUAL SETBACK EXAMPLES

NOTE: FINAL SETBACKS ARE DETERMINED AT ARCHITECTURE AND SITE REVIEW.

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3. DESIGN GUIDELINES

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3. DESIGN GUIDELINES

This section outlines design guidelines for future development that is reflective of the North 40 Vision Statement and Guiding Principles. Mandatory regulations are denoted by the use of the word “shall.” A guideline, which is denoted by the use of the word “should,” is not mandatory, but is encouraged with some discretion by staff, the Planning Commission, and the Town Council. Guidelines include:

- Commercial Design Guidelines
- Residential Design Guidelines
- Neighborhood Identity
- Signage Design Guidelines

In an effort to build upon the Towns existing guidelines, relevant portions of the Town Commercial Design Guidelines and the Affordable Housing Overlay Zone Design Guidelines (AHOZDG) documents have been replicated within this Specific Plan. It is the intention that the Specific Plan goals, policies and guidelines supersede the existing Town documents, but that their intent and applicable concepts not be lost.

3.1 ARCHITECTURAL AND SITE CHARACTER GOALS AND POLICIES

Goal: *To continue the small town character of Los Gatos while enhancing its sense of place and community identity within the Specific Plan Area.*

Policy DG1: Neighborhood Design

Promote a healthy, safe, and secure walkable neighborhood environment.

Policy DG2: Neighborhood Identity

Create a new neighborhood that has its own identity yet complements the existing character of Los Gatos.

Policy DG3: Mixed-Use Projects

Design mixed-use projects to create a pedestrian-scale environment through appropriate street and sidewalk widths, block lengths, relationships of buildings to streets, and use of public spaces.

Policy DG4: Adjacent Neighborhoods

Consider impacts of non-residential development adjacent to residential neighborhoods through screening, buffering, circulation, lighting and placement of utility elements.

Policy DG5: Residential Siting

Locate residential development to minimize traffic, noise, and air quality impacts and encourage walkability to neighborhood serving uses.

Policy DG6: Architecture

Produce high quality, authentic design, and 360 degree architecture consistent with the architectural design guidelines contained within this Specific Plan.

Policy DG7: Historic Character

Design the architecture and landscape to reflect the historic and agricultural heritage of the site and the Town of Los Gatos. The HPC should work with applicants on their individual applications to determine how the agrarian feel of the Specific Plan Area is integrated into the development. The existing red barn should be retained as much as salvageable.

Policy DG8: Wayfinding

Provide a wayfinding signage program for the Specific Plan Area that includes an information kiosk on the area’s history and Downtown businesses and events.

Policy DG9: Project Identification Signage

Design Specific Plan Area signs and gateway elements to reflect the Town’s character and to be consistent with Signage Guidelines in this Specific Plan.



Plazas and fountains can be used to create focal points and identity.



Outdoor dining areas add vitality to streetscape.

3.2 COMMERCIAL DESIGN GUIDELINES

The following guidelines apply to all commercial and mixed-use development in the Specific Plan Area.

3.2.1 SITE PLANNING AND DESIGN

- a. Buildings should be placed close to, and oriented toward, the street. Prominent architectural features should be located near corners and intersections.
 - i. The intent of this guideline is to maintain retail continuity along block fronts in support of a strong pedestrian and retail environment.
 - ii. An exception may be considered for restaurants with outside dining areas. When adjacent to a sidewalk or street right-of-way, a distinguishing barrier or feature is needed to separate the dining area from the right-of-way. Plastic furniture shall not be used.
- b. Plazas, fountains, public art, and vertical building features should be used to create focal points and identity, if feasible.
- c. Functional and aesthetic vehicular and pedestrian connections to adjacent sites should be considered during site plan development.
- d. Natural sunlight and views should be considered when siting buildings and landscaped open spaces.
- e. The visual impact of large parking lots shall be minimized by screening and grade separators and/or locating parking lots behind or to the side of buildings.

3.2.2 OUTDOOR SPACES

- a. Outdoor space should be designed as a purposeful and recognizable area that reflects careful planning and should not be a result of “left over” areas between structures. Defined outdoor spaces should be incorporated into the overall building and project design.
- b. Pedestrian-oriented squares, courtyards, arcades, atriums, verandas, balconies, and roof terraces, should be strategically placed and designed to encourage day and/or night use in the most desirable locations.
- c. Site amenities, such as benches, drinking fountains, provisions for bicycles, water features, and public art, should be utilized and should complement the project’s architectural character.
- d. Pedestrian links should be provided between buildings, common open spaces, and parking areas and should be visually enhanced through the use of landscaping or trellis features, lighting, walls, and/or distinctive paving.
- e. Shade trees that provide relief from the sun should be incorporated within common outdoor spaces.
- f. Incorporate stormwater runoff treatment areas into open space layout.



Site amenities unify streetscape.



Pedestrian linkages between activity areas.



Provide areas for social interaction.



Trellis structure provides shade and helps define space.

- g. Generally, public/common space should satisfy the following criteria:
- Provide areas for informal meetings and social interaction or areas for passive and active uses as appropriate.
 - Be accessible to residents.
 - Provide a strong image and sense of place.
 - Be a part of the pedestrian linkage throughout the development and adjacent land uses to create an interconnected open space system.
 - Provide an overall theme and visual connection between spaces and uses within the development.
 - Include areas of various sizes and configurations throughout the development.
 - Be designed or located to ensure that it is usable year-round by providing areas that have awnings, wind breaks, sun shade, and/or landscaping that can provide shelter from the elements.
 - Create a pleasant pedestrian environment.
 - Support commercial retail activity.
- h. Outdoor dining uses are encouraged.
- i. Design wall edges and landscaping to be compatible with the building design.

3.2.3 PARKING LOT DESIGN AND SCREENING

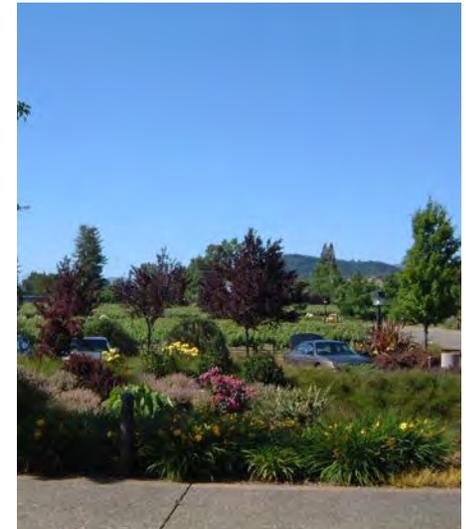
- a. The physical and visual impact of parking should be minimized.
- b. Divide larger parking areas into smaller segments with blocks of landscaping.
- c. Reciprocal access between adjacent parking areas should be provided where feasible so that vehicles are not required to enter the street in order to move from one area to another on the same or adjacent sites.
- d. Parking lots shall be heavily landscaped.
- e. Parking lot edges at property or setback lines should include landscape islands with large species trees and low plants to visually break up long lines of parked vehicles.
- f. Pass-through pedestrian walkways from rear parking lots may be required at the discretion of the deciding body. Pass-throughs should include landscaping, trellises, lighting, and other elements that enrich the pedestrian experience.
- g. Encourage use of pervious pavements and incorporate bio-retention treatment areas into parking lot design.



Low hedge and orchard-style trees integrated into parking lot.



Raised pedestrian connections and landscaped planting areas are encouraged.



Planted berm helps screen parking lot.



Board and batton siding and pitched roof are characteristic of heritage structures.

3.2.4 ARCHITECTURAL STYLE

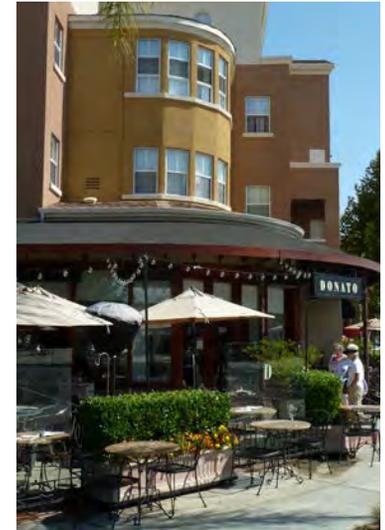
- a. These guidelines are not intended to establish or dictate a specific style beyond the desire to maintain the Town's small town character and attention to human scale and detail.
- b. Proposals for new commercial structures should be developed within the context of Los Gatos' heritage, and the historic and agricultural heritage of the site.
- c. Projects shall be designed to meet their functional needs, but will be expected to reflect Los Gatos' unique qualities of small scale, pedestrian friendliness, and attention to architectural detail.
- d. The use of corporate "chain" architecture detracts from the unique character of the community and is strongly discouraged. Corporate tenants should design their buildings to fit the scale and character of the community.
- e. The Town will work with applicants to adapt critical functional features of prototype plans to their Los Gatos sites, but will not accept standard building forms, elevations, materials, or colors that do not relate to the site, adjacent development, or Los Gatos' community character.

3.2.5 BUILDING FORM

- a. Exterior wall planes should be varied in depth and/or direction. Desirable massing includes:
 - Variation in the wall plane (projecting and recessing elements).
 - Variation in wall height.
 - Roofs containing different forms and located at different levels.
- b. Minimize blank walls by providing one or more of the following:
 - Adding window openings and/or entrances and other relief.
 - Providing recessed glazing and storefronts.
 - Adding vertical pilasters which may reflect internal building structure.
 - Changing color and texture along the wall surface.
 - Varying the planes of the exterior walls in depth and/or direction.
 - Adding trims, projections, and reveals along different wall surfaces.
- c. Structures over one story shall be designed to minimize their visual bulk and relate to the human scale of pedestrians on the street.
- d. Reinforcement of retail linkages along retail-oriented side streets shall be encouraged wherever possible. One good way of accomplishing this is with the use of corner entries and adjacent display windows on both street frontages.



Variation in wall planes add interest to building massing.



Wall and roof planes and forms vary to break up massing of architecture.



Varied roof forms and elements create visual interest.



Retail is oriented to side streets and paseos.



Wall articulation on paseo enhances pedestrian experience.

- e. Similar to the area designated as a C-2 Zone in the Zoning Ordinance, commercial areas should be designed with the following design principles in mind:
- Maintain and enhance the pedestrian orientation
 - Maintain retail and visual continuity
 - Maintain and encourage diversity
 - Provide for visual interest and richness
 - Keep signs subdued in recognition of the strong pedestrian orientation of the street

3.2.6 BUILDING ELEMENTS AND ARTICULATION

- a. Provide architectural elements, detailing and ornament to add richness and variety to building facades and façade depth and detail.
- i. Express columns and beams on the building's exterior.
 - ii. Provide a number of façade layers (e.g., front of columns or pilasters, wall plane, window frame, and window glass).
- b. Provide a unified design around all sides of buildings.
- i. Maintain continuity of design, materials, color, form, and architectural detail for all elevations of a building that are visible from public and common areas or adjacent residences.
 - ii. Where continuity of design is difficult to achieve, substantial landscaping should be provided to screen the area.

- c. Locate service areas so they are not visible from Primary Streets.
- d. When blank walls are unavoidable, add pilasters, trellises, and/or lattices along with landscaping to make the facades more attractive.
- e. Projects located on corner parcels at signalized intersections along Lark Avenue and Los Gatos Boulevard should incorporate major design features on the intersection corner.
 - i. Buildings located at these corner locations are strongly encouraged to frame and front onto intersections. Special care shall be taken to avoid obstructing views to the surrounding hills.
 - ii. Other physical improvements such as fountains, special landscaping or other unique improvements are encouraged.
 - iii. All corners should have special landscaping incorporating flowering plants.
- f. Provide well defined project and building entries.
- g. Design all projects with a strong commitment to human scale.
- h. Provide architectural details to enhance the visual interest of facades. Elements that are encouraged include the following:
 - Projecting cornices with decorative moldings and/or brackets
 - Shaped parapets
 - Planter boxes
 - Projecting molding
 - Inset medallions



Facade layers add richness and variety to the building's exterior.



Balconies, roof awning and windows integrated into building design.



Trellis structure and landscaping can enhance blank walls.



Design elements continue around all sides of the building.



A solid bulkhead below the window is desirable.



Awnings help define entries.



Individually articulated tenant spaces are encouraged to appear as several smaller buildings.

- i. Provide vestibules at building entries.
 - i. Recess entry doors three to six feet from the front facade.
 - ii. Pave vestibule floors with tile, brick, stone, or a similar high quality hard surface to set the vestibule apart from the sidewalk.
- j. Emphasize display windows and storefront entries.
 - i. Provide interesting storefronts with prominent display windows.
 - ii. Traditional storefronts with bulkheads below the windows and glass on both the street front and the sides of vestibules are desirable. However, larger and more contemporary display windows are also acceptable, but should have some solid base at least 12 inches high between the bottom of the window and the paving below.
 - iii. Use bulkhead materials that are consistent with the building's materials and traditional to Main Street-style storefronts (e.g., stucco, tile, stone, wood panels.) The decorative treatment of bulkheads is encouraged (e.g., wood moldings over a wood base panel, decorative tile patterns.)
 - iv. Window box planters below display windows are encouraged to add color and visual interest.
- k. Maintain a high degree of transparency at all window areas.

- l. Avoid dark or highly reflective glazing.
- m. Display window lighting to enhance the nighttime vibrancy is strongly encouraged. Methods may include shielded or recessed spotlights to highlight display merchandise or pin lights used to define the outline of windows.
- n. Display windows should be used as display windows only and not covered up from the inside.
- o. Operable windows and french doors are encouraged for restaurants and coffee cafes.
 - i. Window types and proportions should be complementary to the architecture and design of the facade.
- p. Install awnings and trellises when weather and sun exposure protection are desired.
 - i. Relate awning placement to the major architectural elements of the facade.
 - ii. Avoid covering any transom windows, architectural elements such as belt courses, decorative trim and similar features.
 - iii. Separate awnings over individual storefront bays defined by the columns or pilasters are encouraged rather than placing a continuous awning across the building frontage.



Windows provide transparency on the ground floor.



Awnings and window box planters can add color and visual interest.



Trellis structure provides protection from sun and articulation to facade.



Contemporary metal awning defines window and entry to cafe.



Fabric awnings are encouraged.



Awning and typeface should vary in size and color among tenants.

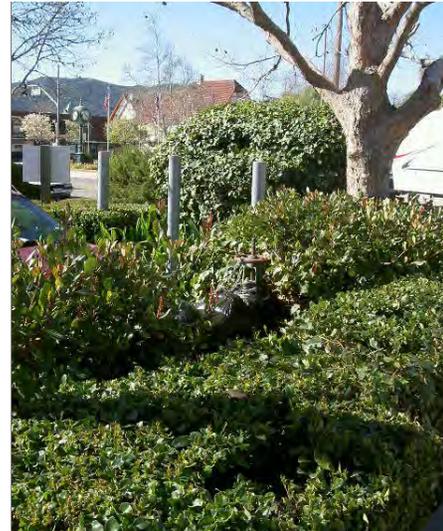


Separate awnings over windows and doors are encouraged.

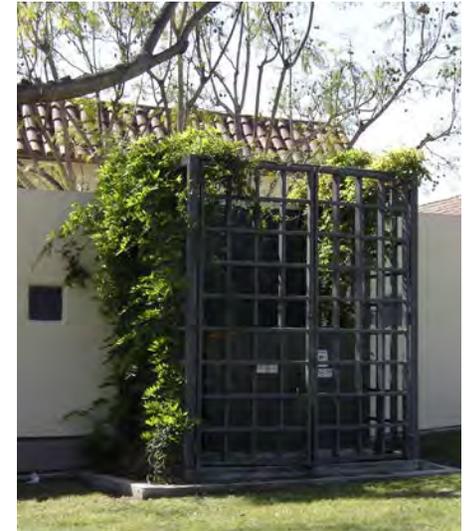
- iv. Place awnings and canopies at elevations that relate to the height of pedestrians and provide a sense of shelter. Use awning appropriate to the building style.
- v. Avoid shiny fabrics.
- vi. Backlit awnings that visually appear as large light sources are not permitted.
- vii. Patterned and striped awnings are discouraged. If patterned awnings are proposed, the burden will be on the applicant to demonstrate that the colors and pattern are appropriate for the design and color of the building
- viii. Operable awnings are encouraged when appropriate for the style of the building.
- q. Utilize high quality storefront materials.
- r. Wood window frames are encouraged. Alternatively, vinyl covered or dark anodized aluminum metal storefront window and door frames are suggested. Anodized aluminum frames are not generally desired, but may be considered on a case-by-case basis.
- s. Strong building colors that are used for branding or advertising purposes may not be approved.

3.2.7 UTILITY ELEMENTS

- a. Any outdoor equipment, whether on a roof, side of a structure, or the ground should be appropriately screened from view. The method of screening should be architecturally integrated with the adjacent structure in terms of materials, color, shape, and size.
- b. Utility service areas, such as electrical panels, should be placed within enclosures that are architecturally integrated into the building design.
- c. Access for fire apparatus should be part of the planning process and every attempt should be made to work with the fire department to locate and/or screen the apparatus so as not to disrupt the visual integrity of a project.
- d. Drainage should be directed to permeable areas such as yards, open channels, or vegetated areas, avoiding discharge to roads and minimizing discharge to the storm drain system.
- e. Lighting levels of outdoor lighting should not draw attention to the light source. The intensity level of light as measured in footcandles, and the type of bulb or source should be carefully addressed.
- f. Spotlighting or glare from any site lighting should be shielded from adjacent properties and directed at a specific object or target area.
- g. Exposed bulbs should not be used. Cut-off lighting is required.
- h. Parking lot light standards should not exceed a 20 foot maximum mounting height.



Landscaping can be used to buffer utilities from view.



Screens, trellises, or walls can be used to buffer utilities from view.



Service area integrates wall and planted Greenscreen. Photo courtesy: Greenscreen



Vines on Greenscreen will hide utilities from view.



Integrate compatible architectural elements to screen trash.



Low wall is complementary to building design.

- i. Shoestring lighting may be used to accent outdoor seating or key areas.
- j. Vehicle sight distances should be considered when selecting and placing landscaping, fencing, and signage.
- k. Integrate the screening for all trash and service areas into the design of the buildings.
 - i. During the Architecture and Site application process, the placement of trash and service areas should be shown on the plans. Locate trash and service areas away from residential uses to avoid odor and noise conflicts.
 - ii. Avoid walls that appear to be tacked onto the main structure in favor of walls that integrate the service walls into the overall design of the building complex.
 - iii. Match wall materials to that of the building.
 - iv. Where screen walls are prominently visible, provide additional detail appropriate to the design of the main structure (e.g., wall caps similar to those on a primary structure).
 - v. In many cases, the use of a lattice work with dense, flowering vines may be used. In those cases, a lesser building material (e.g., concrete block) may be used if the lattice work will provide a strong texture while the landscaping is growing to maturity, and if it can be demonstrated that the landscaping selected will provide a green screening of the walls within two years of installation.

- vi. Trash enclosures in commercial areas should be covered and connected to the Sanitary Sewer System to meet Town water quality requirements.
- l. Provide visual buffering of on-site utility elements.
 - i. Locate transformers, valves and similar elements where they will be least visible from public views and rights-of-way. If not possible, these elements should be placed underground or, at a minimum, screened from view with walls and landscaping that relate to the remainder of the project.
 - ii. Utilize landscaping and/or walls to screen transformers and other utility elements if they must be located in close proximity to the street right-of-way.
 - iii. Applicants will be required to submit a site plan at the time of submittal for review and approval to show the location, type and size, including height, of all utility elements to be located on the parcel.
- m. Screen all roof top mechanical equipment.
 - i. Integrate equipment into roof forms, whenever possible. If equipment cannot be located in this manner, consider alternatives such as interior mechanical rooms and under grounding.



Decorative wall enhances utility enclosure.



Raised planters buffer sidewalk from traffic.



Orchard planting reflects North 40 heritage and helps to define the path's edge.



Landscaping helps define a district's identity.

3.2.8 RESPECT THE PRIVACY OF NEIGHBORING RESIDENTS

- a. Avoid windows which would provide views into residential private yard spaces.
- b. Keep window sizes small on facades facing residences where windows can be seen to minimize lighting intrusion.
- c. Provide shielding for any exterior lighting visible from neighboring residential uses.
- d. Avoid placement of mechanical equipment where noise would negatively impact residential neighbors.
- e. Visually screen all trash and outdoor storage areas from view.

3.2.9 PROJECT LANDSCAPE AND HARDSCAPE

- a. All projects shall be well landscaped.
 - i. Landscaping should relate to existing landscape treatments along the adjacent street fronts.
 - ii. Smaller landscaping (e.g., planter pots, window boxes) are used often within the Town to provide smaller scale elements close to the pedestrian, and should be included within commercial projects whenever possible.
 - iii. Landscaping should have form and substance to define edges and paths, to provide visual focal points, and to buffer less desirable views (e.g., less finished facades facing public ways or residences.)

- b. A licensed landscape architect shall certify that all landscape plans include drought tolerant plants and meet Water Efficient Landscape Ordinance requirements.
- c. Landscaping shall be used to soften the appearance of buildings and to integrate new construction into the overall commercial/residential neighborhood. Where space is inadequate for in-ground planting, use container plantings.
- d. Integrate landscape elements that reflect the orchard heritage of the area. Where portions of the existing orchards are to be retained, the applicant should work with an arborist and the Historic Preservation Committee to determine if original trees could be retained or if they should be replanted.
- e. Soundwalls along the adjacent highways may be treated in an artistic manner appropriate to this special site and the Town of Los Gatos.
- f. Landscape areas should be provided between the soundwall and the highway. Details of the soundwall and landscaping shall be provided during the Architecture and Site Review approval process. Gate(s) shall be provided in the soundwall to access the landscape area adjacent to the highway for maintenance purposes.



Enhanced soundwall treatment.

3.2.10 SUSTAINABILITY GUIDELINES

- a. Promote use of native and/or drought tolerant plants in parking lot islands and other landscaped areas, where feasible.
- b. Utilize natural drainage systems to the maximum extent practical.
- c. Minimize impervious area to the maximum extent practical.
- d. Non-structural Best Management Practices (BMP's) should be used unless they are infeasible in which case the infeasibility should be documented and structural BMP's implemented.
- e. Pre-treat stormwater as currently defined by National Pollutant Discharge Elimination System (NPDES) Guidelines prior to infiltration or discharge from site.
- f. Site development should comply with all applicable Regional Water Quality Control Board and Santa Clara County regulations for water quality and quantity.
- g. Design buildings with pedestrian oriented building entries facing onto common space, streets, paseos and plazas.
- h. Design neighborhoods and projects with strong pedestrian connections to sidewalks, parks, pathways, and existing or future bike facilities.
- i. Include bicycle parking facilities and on-site showers in major non-residential development projects.
- j. Encourage sustainable building practices to reduce energy use through solar orientation that takes advantage of shade, prevailing winds, landscaping and sun screens.
- k. Design new buildings and parking lots to include or allow for the easy, cost-effective installation of future solar energy systems, where feasible.
- l. Encourage the Integration of community gardens and urban farm sites into neighborhoods where appropriate.
- m. Refer to the Los Gatos Sustainability Plan for additional guidelines.



Solar panels can double as parking lot shade structures.

3.3 RESIDENTIAL DESIGN GUIDELINES

Residential products should be designed to meet the unmet needs of the community and provide a mix of market rate and affordable housing in a multi-family setting. The following guidelines apply to the following residential product types:

- Cottage Cluster
- Townhomes/Garden Cluster
- Rowhouses
- Multi-family
- Condominium
- Live/Work Loft



Pedestrian connections should be provided within residential areas.



Opportunities for social interaction creates a desirable sense of community.



A community garden is an amenity to a neighborhood.



Stoop provides individual unit identity.



Pedestrian walkways create linkages throughout the Specific Plan Area.

3.3.1 SITE PLANNING AND DESIGN

- a. Relate buildings to the street and locate them on the site so that they reinforce street frontages.
- b. Ensure that all buildings have well-designed and visible entries.
- c. Provide each unit with its own visual identity, whenever possible.
- d. Provide pedestrian accessibility to adjacent uses with paseos, gates, pedestrian walkways, crossings, etc.
- e. Locate buildings and landscaping to maximize solar access during cooler months and to control it during warmer months. Optimize natural ventilation, sunlight, and views for each unit.
- f. Enhance access to transit stops whenever possible.
- g. Orient unit entries to streets and common open space rather than a parking courtyard.
- h. Minimize the number of curb cuts and street-facing garages.
- i. Avoid fences over three feet tall along public street frontages.

- j. Link internal unit entries to adjacent streets with clear open space and pedestrian circulation networks.
- k. Provide landscape screening, which may include a fence up to six feet in height as a visual buffer along property lines of any adjacent residentially zoned lot.
- l. Avoid turning unit back elevations and patio walls to public streets.
- m. If parking is not attached to the units, utilize small parking areas reasonably close to the living units. Break large parking areas and aisles into smaller segments with substantial landscaping.



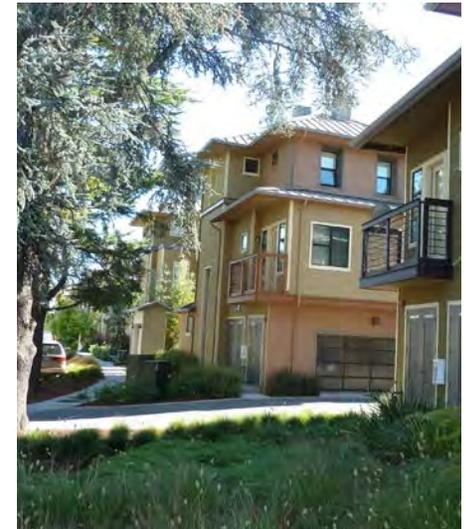
Building entries oriented on common open space areas is encouraged.



A well defined pedestrian circulation network links units to shopping and amenities.



Pathways between units provide linkages through the Specific Plan Area.



Parking can be oriented internally to allow units to front the street.



Trees and shrubs should be used in parking lots to provide shade and soften overall impact.



Permeable surfaces can be used to delineate parking spaces.

3.3.2 PARKING DESIGN AND SCREENING

- a. Place parking lots at the rear or on non-street side of the site to allow a majority of dwelling units to front on the street.
- b. Plant trees and shrubs to soften the overall impact of parking areas and to provide shade.
- c. In developments without garage parking, bicycle parking will be required per Santa Clara Valley Transportation Authority (VTA) standards.
- d. Below grade parking is encouraged with entries placed at the rear or sides of the structures whenever possible. They should be recessed as much as possible from the building façade, – especially where security gates are used at the garage entry.
- e. Podium garages should not extend more than six feet above grade along 'A' Street, Neighborhood Street, Noddin Avenue, Burton Road or common open spaces of pedestrian walkways unless fully screened by building walls with decorative treatment and detail.
- f. Projects constructed on top of parking podiums should take special care to provide design elements to minimize the hard edge of the parking podium. Decks extending beyond the podium edge and varied setbacks for the residential units are just two ways of approaching this issue.
- g. Parking may be provided in small groupings of surface lots, carports, garages or a combination of the above.

- h. Textured decorative paving in driveways visible from the street is strongly encouraged.
- i. Guest parking should be distributed throughout the development.
- j. Entry driveways should have strong landscaped edges with terminus view focused on landscaped areas or building entries, not parked cars.
- k. The edges of any garage structure and vents into the garage visible above grade should be screened with evergreen plant materials. Earth berms and other techniques to tie the top of the garage structure into the surrounding grade level should be utilized.
- l. Garages and parking areas should be located to minimize the visual impact on the street.
- m. When viewed from the street, garages should be subordinate to the main living area. Where possible, the garage should be recessed behind the dwelling unit and not located between the main living area and the street.
- n. Garage doors should be recessed into, rather than flush with, the exterior wall.
- o. Detached garages and accessory structures should be designed as an integral part of the architecture of the project and should be similar in materials, color, and detail to the principal structures of a development.
- p. Garage doors should feature windows, recesses, and moldings to help blend the garages with the character of the unit.



Parking tucked behind buildings on alleys allow units to face street.



Trellis feature over garage door and recessed door help articulate garage door.



Units have direct views of common open space.



Patio and porch provides private open space for individual units.

- q. Where there are adjacent garages, provide a landscaped area to separate them and reduce the amount of driveway paving.
- r. Detached garages and accessory structures should incorporate roof slopes and materials similar to the principal structures of a development. Flat roofs are discouraged.

3.3.3 COMMON OPEN SPACE

- a. Residents of housing developments should have safe, efficient, and convenient access to usable open space, whether public or private, for recreation and social activities.
- b. Convenient access to common open space and adjacent mixed-use development should be incorporated into the project by way of pedestrian pathways and bicycle access.
- c. Open space should focus on areas that are usable to the residents and not merely remainder parcels with marginal utility.
- d. Locate common open spaces so that they can be viewed from individual units.
- e. Provide energy-efficient lighting.

3.3.4 PRIVATE OPEN SPACE

- a. Provide each household with some form of useful private open space, such as a patio, porch, deck, balcony, yard, or shared entry porches or balconies.
- b. Private open space should be easily accessible – physically and visually – from individual units.

3.3.5 LANDSCAPING

- a. Design landscaping to enhance the architecture and create and define useful common and private spaces.
- b. Use hardy, native plant species – trees, shrubs, groundcover – that are easy to water and maintain, and conform to water efficiency standards.
- c. Shade paved areas, especially parking lots.
- d. Provide a variety of seating in landscaped areas, where appropriate.
- e. Provide appropriate lighting to accentuate landscaping and insure that paths are safe at night.

3.3.6 BUILDING FORM AND ARTICULATION

- a. Design buildings for the site.
- b. Eliminate box-like forms with large, unvaried roofs by using a variety of building forms and roof shapes with clusters of units, and variations in height, setback, and roof shape.
- c. Make the building visually and architecturally pleasing by varying the height, color, setback, materials, texture, landscaping, trim, roof shapes, and ridge orientation for all elevations.
- d. Structures should be varied in height, size, proportions, orientation and roof lines.
- e. Emphasize entries by adding projecting porches or other entry elements.



Landscaping should be used to help define common space and frame architecture.



A variety of roof forms help eliminate box-like architecture.



Varied building heights, setbacks, and materials help enhance building facade.



Variations in building height and setbacks help break up building mass.

- f. Emphasize the individuality of each unit with well-defined limits and individual entries and details.
- g. Elevations should be mixed within a development to avoid repetition of identical facades and roof lines.
- h. Add variety to second and third floors with varied eave heights, windows and ridge line variations.
- i. Provide horizontal and vertical wall plane offsets to break up the building mass. Avoid building forms that appear to be large boxes with elements attached to them.
- j. Provide buildings with a well-defined base, middle, and top to reduce the apparent building height and bulk. Significant projecting roof overhangs are strongly encouraged.
- k. Carry wall materials, window types and architectural details around all sides of the building. Avoid side and rear elevations that are markedly different from the front elevation.
- l. Architectural features (e.g., bay windows, chimneys, canopies, cornices, awnings) are encouraged.
- m. Use porches, stairs, railings, fascia boards, and trim to enhance a buildings' character.
- n. Provide pedestrian-oriented elements and details on facades facing sidewalks. Elements such as projecting balconies and awnings can add visual interest and richness to the street environment.
- o. Provide door and window openings with sizes and proportions that are appropriate to the buildings architectural character.

- p. Structures should include building articulation, changes in materials or textures, or other architectural features such as:
- Horizontal and vertical wall plane changes
 - Varied roof forms and orientations
 - Bay windows
 - Roof dormers
 - Material and color changes
 - Applied decorative features
 - Roof segments over windows
 - Metal or wood balcony railings
 - Planter boxes and plant rings
- q. Window articulation, such as sills, trim, kickers, shutters, or awnings, should be applied, where appropriate to the architectural style, to improve the facade of the building.
- r. Any faux shutters should be proportionate to the adjacent windows to create the appearance of real and functional shutters.
- s. Material changes should occur at a change in plane where the changes tend to appear substantial and integral to the structure, preferably at an inside corner. Material changes not occurring at a change in plane appear “tacked-on” and should be avoided.



Window articulation should be integrated into the facade.



Faux shutters should be proportionate to the window to appear as functional shutters.



A variety of colors and architectural building forms should be incorporated into the building's design.



Mechanical equipment should be architecturally integrated into the building's design.

3.3.7 UTILITY ELEMENTS

- a. Any outdoor equipment, whether on a roof, side of a structure, or the ground should be appropriately screened from view. The method of screening should be architecturally integrated with the adjacent structure in terms of materials, color, shape, and size.
- b. Utility service areas, such as electrical panels, should be placed within enclosures that are architecturally integrated into the building design.
- c. Utility equipment should be screened with solid masonry walls, berms, and/or landscaping.
- d. Transformers required to be installed adjacent to the street should be placed underground, where feasible.
- e. Satellite dishes should not be mounted on the roof or be visible from the street, where feasible.
- f. Access for fire apparatus should be part of the planning process so as not to disrupt the visual integrity of a project.
- g. Gutters and downspouts on the exterior of the building should be decorative or designed to integrate with the building facade.
- h. Drainage should be directed to permeable areas such as yards, open channels, or vegetated areas, avoiding discharge to roads and minimizing discharge to the storm drain system.
- i. Common mailbox enclosures should be designed to be similar in form, materials, and color to the surrounding buildings.
- j. Every development shall provide a trash and recycling enclosure that is capable of handling the refuse generated by that site.
- k. At least half of the trash and recycling area should be dedicated to recycling containers.
- l. Trash enclosures serving more than one residential unit shall be covered and connected to the Sanitary Sewer System to meet Town water quality requirements.
- m. Trash and recycling enclosure should be consistent with the design of the project and the building architecture. Similar or the same materials should be used on the enclosure as those on the surrounding buildings. Architecturally designed roof structures should be used to create a finished looking structure.

- n. A pedestrian entrance to the trash and recycling enclosure should be provided to minimize opening of large access gates.
- o. Trash and recycling enclosures should be separated from adjacent parking stalls by a minimum three-foot wide planter with low-growing plant materials to ensure that adequate space is available for passengers to access a vehicle in an adjacent parking space.
- p. Drainage from adjoining roof and pavement should be diverted around the trash and recycling area.
- q. Lighting levels of outdoor lighting should not draw attention to the light source. The intensity level of light as measured in footcandles, and the type of bulb or source should be carefully addressed.
- r. Spotlighting or glare from any site lighting should be shielded from adjacent properties and directed at a specific object or target area.
- s. Exposed bulbs should not be used. Cut-off lighting is required.
- t. Pedestrian lights should not exceed a 15 foot maximum mounting height.
- u. Vehicle sight distances shall be considered when selecting and placing landscaping, fencing, and signage.



Mailbox enclosures should reflect project architecture.



Trash and recycling enclosures should be screened.



Gates on trash enclosure and trellis above have compatible colors.

3.3.8 SUSTAINABILITY GUIDELINES

- a. Promote use of native and/or drought tolerant plants in parking lot islands and other landscaped areas, where feasible. A licensed landscape architect shall certify that all landscape plans include drought tolerant plants and meet Water Efficient Landscape Ordinance requirements.
- b. Utilize natural drainage systems to the maximum extent practical.
- c. Minimize impervious area to the maximum extent practical.
- d. Non-structural Best Management Practices (BMP's) should be used unless they are infeasible in which case the infeasibility shall be documented and structural BMP's implemented.
- e. Pre-treat stormwater as currently defined by National Pollutant Discharge Elimination System (NPDES) Guidelines prior to infiltration or discharge from site.
- f. Site development should comply with all applicable Regional Water Quality Control Board and Santa Clara County regulations for water quality and quantity.
- g. Design buildings with pedestrian oriented building entries facing onto common space, streets paseos and plazas.
- h. Design neighborhoods and projects with strong pedestrian connections to sidewalks, parks, pathways and existing or future bike facilities.
- i. Include bicycle parking facilities and on-site showers in major non-residential development projects.
- j. Encourage sustainable building practices to reduce energy use through solar orientation that takes advantage of shade, prevailing winds, landscaping and sun screens.
- k. Design new buildings and parking lots to include or allow for the easy, cost-effective installation of future solar energy systems, where feasible.
- l. Encourage the integration of community gardens and urban farm sites into neighborhoods where appropriate.
- m. Refer to the Los Gatos Sustainability Plan for additional guidelines.



Example of a bioretention area.

3.4 NEIGHBORHOOD IDENTITY

The Specific Plan Area is a unique site within the Town of Los Gatos, and within the region. By utilizing quality signage, architecture, and identity elements, the Specific Plan Area can:

1. Create a gateway statement for Northern Los Gatos.
2. Incorporate architectural, landscape, and signage elements to unify the Specific Plan Area as a new neighborhood with its own identity while complementing the existing Town character and charm.
3. Connect this part of Los Gatos to the rest of town.
4. Reflect the agricultural history of the site.

In addition to signs and wayfinding elements, the Specific Plan Area should be treated with a unique image, or “brand,” appropriate to its history and relationship to the Los Gatos community. Together, signage, architectural, landscape and urban design elements can all contribute to branding the Specific Plan Area neighborhood. Branding elements include building materials and forms, trees and landscape treatments, street lights, benches, trash cans, planters, walls, and signage.

3.4.1 SPECIFIC PLAN AREA INSPIRATION

The Specific Plan Area brand and neighborhood identity elements should draw from authentic materials and patterns of Los Gatos and the agricultural history of the site. These include natural stacked stone walls, vines and plantings on walls, orchards, wrought iron gates, wood ranch style gates, and barn material and forms. Combining elements that reflect the history and character of Los Gatos and the site is key to creating the Specific Plan Area identity.



Inspiration images: Brick and stone walls, wrought iron gates, agricultural gate, Los Gatos cat and orchard.



Street furniture can be a unifying element to a neighborhood.

Neighborhood identity should also be reinforced along Los Gatos Boulevard with sculptural, topiary, or other landscape treatments. The Los Gatos Boulevard frontage, orchestrated with attractive patterns and rhythms of materials and elements, creates a dual opportunity of identifying the Specific Plan Area neighborhood and creating a welcoming new gateway into the Town. Similar materials and elements should be strategically integrated within the site, along with interpretive signage that celebrates the history of the land to add additional rich layers of meaning for visitors and residents.

3.4.2 STREET FURNISHINGS

Street furnishings can be a strong unifying element for a neighborhood or area. Street furnishings should be provided along the streets and within plazas and paseos throughout the Specific Plan

Area and along Los Gatos Boulevard and Lark Avenue frontages. Consistent lighting, benches, waste receptacles, bicycle racks, and tree grates should be installed throughout the Specific Plan Area to help give the neighborhood a unified feel and avoid the appearance of a piecemeal approach to the design.

3.4.3 STREETScape LANDSCAPING

Planting strips and street trees add to a neighborhood's identity and make an important contribution to the appearance of an area. There are a number of trees that have been identified in the following table as appropriate in key locations within the Specific Plan area. Tree planting and placement shall maximize the health and viability of the tree while avoiding damage to the sidewalk, curb, and gutter.

Street tree spacing should be determined by the expected mature size of the tree. Generally, trees should be planted with the following spacing:

- a. Small trees (<20 feet crown width at maturity) should be planted approximately 15 feet on center.
- b. Medium sized trees (20 to 35 feet crown width at maturity) should be planted approximately 25 feet on center.
- c. Large trees (>35 feet crown width at maturity) should be planted approximately 35 feet on center.
- d. Generally, trees with narrow crowns (less than 20 feet at maturity) should be planted at closer spacing while trees which normally develop broad crowns (> 40 feet at maturity) should be afforded wider spacing (e.g., 40 to 50 feet on center).

3.4.4 LANDSCAPE PALETTE

TABLE 3-1 TREES			
BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH
PERIMETER TREES			
PINUS RADIATA	MONTEREY PINE	80'-100'	25'-35'
POPULUS NIGRA	LOMBARDY POPLAR	40'-100'	15'-30'
SEQUOIA SEMPERVIRENS	REDWOOD	70'-90'	15'-30'
ORCHARD TREES			
JUGLANS CALIFORNICA HINDSII	CALIFORNIA BLACK WALNUT	30'-60'	30'-40'
MALUS FLORIBUNDA "PARRSI"	PINK CRABAPPLE	12'-20'	12'-20'
PRUNUS BLIREANA	PURPLE LEAF PLUM	40'	25'
PRUNUS CERASIFERA	CHERRY PLUM	10'-18'	10'-18'
PRUNUS KWANSAN OR AKI BONO	JAPANESE CHERRY	40'	25'
PRUNUS PERSICA	PEACH	10'-18'	10'-18'
PRUNUS PERSICA NUCIPERSICA	NECTARINE	40'	25'
PRUNUS SALICINA	JAPANESE PLUM	10'-18'	10'-18'
PYRUS CALLERYANA	CALLERY PEAR	40'	25'
COMMON OPEN SPACE TREES			
ACER SANGO KAKU	CORAL BARK JAPANESE MAPLE	15'-20'	15'-20'
AESCULUS CALIFORNICA	CALIFORNIA BUCKEYE	10'-20'+	30'+
ARBUTUS MENZIESII	PACIFIC MADRONE	20'-100'	20'-100'
CEDRUS DEODARA	DEODAR CEDAR	80'	40'

TABLE 3-1 TREES

BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH
KOELREUTERIA PANICULATA	GOLDENRAIN TREE	20'-35'	25'-40'
LAGERSTROEMIA SPECIES	CRAPE MYRTLES	15'-25'	20'-25'
LIQUIDAMBAR STYRACIFLUA	SWEET GUM	60'	20'-25'
LIRIODENDRON TULIPIFERA	TULIP TREE	60'-80'	40'
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA	80'	60'
MAGNOLIA GRANDIFLORA "LITTLE GEM"	DWARF SOUTHERN MAGNOLIA	8'	12'-15'
MAGNOLIA SOULANGIANA "STELLATA"	SAUCER MAGNOLIA	12'-18'	6'-12'
MALUS FLORIBUNDA "PARRSI"	PINK CRABAPPLE	12'-20'	12'-20'
PINUS CANARIENSIS	CANARY ISLAND PINE	50'-80'	20'-35'
PINUS PINEA	ITALIAN STONE PINE	40'-80'	40'-60'
PLATANUS RACEMOSA	SYCAMORE	30'-80'	20'-50'
PRUNUS KWANSAN OR AKI BONO	JAPANESE CHERRY	20'-25'	20'-25'
QUERCUS AGRIFOLIA	COAST LIVE OAK	20'-70'	20'-70'
QUERCUS DOUGLASII	BLUE OAK	30'-50'	40'-70'
QUERCUS ILLEX	HOLLY OAK	30'-60'	30'-60'
QUERCUS LOBATA	VALLEY OAK	50'-70'	50'-70'
QUERCUS SUBER	CORK OAK	30'-60'	30'-60'
ZELKOVA SERRATA	SAWLEAF ZELKOVA	60'	60'

TABLE 3-1 TREES

BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH
SPECIMEN TREES			
ACER PALMATUM	JAPANESE MAPLE	20'	20'
BETULA UTILIS JACQUEMONTII	HIMALAYAN BIRCH	40'	20'
CERCIDIPHYLLUM JAPONICUM	KATSURA TREE	40'	25'
CERCIS OCCIDENTALIS	WESTERN REDBUD	10'-18'	10'-18'
STREET TREE			
CINNAMOMUM CAMPHORA	CAMPHOR TREE	50'+	60'
GINKGO BILOBA	MAIDEN HAIR TREE	30'-50'	15'-25'
LAURUS NOBILIS	SWEET BAY	12'-40'	12'-40'
LIQUIDAMBAR STYRACIFLUA	SWEET GUM	60'	20'-25'
MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA	80'	60'
MELALEUCA LINARIIFOLIA	MELALEUCA	20'-30'	20'-25'
TILIA CORDATA	LITTLE LEAF LINDEN	30'-50'	15'-30'
SEE APPENDIX B FOR ADDITIONAL INFORMATION			



Signage adds to ambiance of street scene.

3.5 SIGNAGE

Signage is a key component of forming the neighborhood identity. Implementing these Signage Guidelines for the Specific Plan Area neighborhood will help to realize the General Plan Land Use Element's vision of high-quality architecture and design that reflects the rural and agricultural history of the site. All signage within the Specific Plan Area shall conform to the following guidelines, as well as, the regulations provided within Chapter 2 of this document.

A sign program will be developed as an effective method to establish a unique identity to the Specific Plan Area and adjacent neighborhoods. The sign program should include, but not be limited to, gateways, directional signage, parking signs, street signs, directories (kiosks), and banners.

3.5.1 SPECIFIC PLAN AREA ENTRY FEATURES AND IDENTITY SIGNAGE

The four primary entries into the Specific Plan Area are Lark Avenue at 'A' Street and Neighborhood Street, Noddin Avenue, and Burton Road at Los Gatos Boulevard.

These four entry locations should be designed with elements that unify the Specific Plan Area with the Town of Los Gatos, while at the same time are designed to reflect the desired identity and character of the Specific Plan Area. Entries should have a combination of accent features including landscaping, trees, architectural features, signage, and/or enhanced paving.

Since the Specific Plan Area is striving to be an extension of the adjacent neighborhoods and not a typical strip commercial center or residential project, the approach to the entry treatments and signage should be customized to reflect this unique new mixed-use neighborhood.

Entry signage at these locations should include authentic materials – stone, metal and/or wood in a creative and timeless style. Additional signage requirements are provided in Chapter 2 of this document. Applicants should also refer to Division 3 of the Zoning Ordinance. The guidelines in this chapter supplement the Sign Ordinance and are intended to provide more detail in regard to good signage design principles and community expectations.

The adjacent images are examples of character and materials reflective of the site's agrarian history and appropriate at entry locations to the North 40 neighborhood.



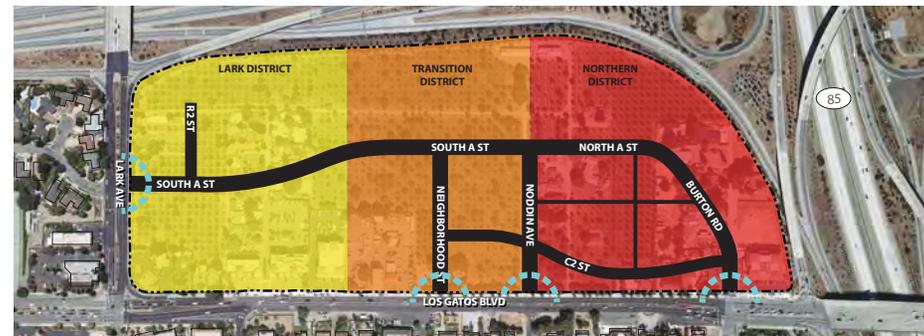
Orchard trees.



Authentic stone materials and elements reflective of the sites agrarian history.



Corten Steel on repurposed wood from historic structure.



Neighborhood entry statement locations.



Projecting sign example.



Hanging sign example.

3.5.2 TENANT SIGNAGE

Tenant signs necessary for identifying businesses are encouraged to be creatively designed in their materials, fonts, illumination, and detail. These guidelines supplement the standards provided within Chapter 2 of this Specific Plan. Signs will be limited to the following types which are illustrated below and described on the following pages.

Tenant Signs

- Wall Signs
- Awning Signs
- Window Signs
- Projecting Signs
- Plaque Signs
- Hanging Signs
- Ground Signs
- Freestanding Signs

3.5.3 MATERIALS

- a. Signs must be made of durable materials that are appropriate and complementary to the building architecture.

3.5.4 SIGN LETTER SIZE

- a. The message of effective signs need to be easily read and understood by passing motorists and pedestrians. A number of factors including distance from the sign, speed of travel, letter-to-background contrast, and the number and nature of nearby competing signs contribute to the “readability” of the sign.
- b. For the purpose of evaluating appropriate sign size, the Town will consider the normal sign viewing distances, the general nature of the street (e.g., width and traffic speed), and the size of existing signs in the area.

3.5.5 WALL SIGNS

- a. Wall signs are panels or individual letters mounted flat against and parallel to a building wall or roof fascia.
- b. Use either individually applied letters to the face of the wall, or apply sign letters to a board or panel mounted on the wall face.
- c. Signs painted directly onto wall surfaces may be allowed if they meet all applicable guidelines contained within this section.
- d. Back-lit letters and signage is permitted.
- e. Exterior illuminated signs should utilize shielded spot lights.



Wall sign example.



Back-lit wall sign example.



Example of wall signage



Sign reflective of type of business

- f. Conceal all sign and sign lighting raceways and other connections.
- g. Sign copy and graphics applied to a board or panel may consist of any of the following:
 - i. Individual letters and graphics of wood, metal or similar materials.
 - ii. Individual letters and graphics carved into or applied to the surface of a wood panel.
 - iii. Letters and graphics painted directly onto the surface of the panel.
- h. Sign height and width should be appropriate to the building on which it is placed and the distance of the sign from fronting streets.
 - i. Adequate amounts of visual open space shall be provided so that signs appear balanced and in scale in relation to their backgrounds and adjacent signage. Up to 75% of the width and 80% of the height of the available architectural background may be used for signage.

3.5.6 AWNING SIGNS

- a. Awning signs consist of letters and graphics applied directly to the face or valence of awnings.
- b. Apply signs to awning front valences (i.e., the flat vertical surface of awnings) or to sloped awning faces.
- c. Limit the signage information on awnings.
 - i. Generally limit awning signs to the business name, business logo, services or type of business (e.g., French Cuisine), and/or the business address number.
 - ii. Limit the size of logos or text placed on awning sloped faces to a maximum of 15% of the sloped surface areas.
 - iii. Limit sign width on awning valences to a maximum of 85% of the awning width.
 - iv. Limit the letter height to a maximum of 80% of the valence height.
 - v. Backlit awnings that make the entire awning a large sign will not be allowed. Signage on the awnings slope face maybe illuminated by shielded and attractive directional spotlights.



Name, logo, and services are clear and uncluttered.



Example of limited signage.



Window sign example.



Projecting signs should vary in style and should reflect the character of the business.

3.5.7 WINDOW SIGNS

- a. Window signs are primarily oriented to passing pedestrians, and are generally applied to display windows.
- b. Window signs should be limited to a maximum of 25% of any individual window.
- c. Consider the use of logos and creative sign type. Graphic logos and images along with special text formats can add personality and interest to window signs.
- d. Use high-quality materials and application methods.
 - i. Window signs shall be created with permanent or fade resistant materials, paint, gold-leaf lettering, or glass etching.
 - ii. Tubular neon suspended behind the window glass.
 - iii. Wood or metal panels with applied lettering.
 - iv. Paper signs placed in windows are not allowed.

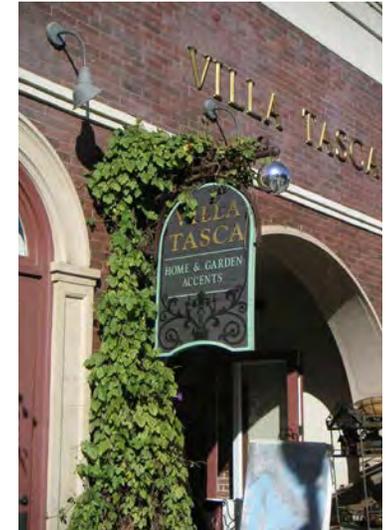
3.5.8 PROJECTING SIGNS

- a. Projecting signs are relatively flat, two-sided panels attached to brackets which are mounted on and perpendicular to the face of buildings and storefronts. They often include graphic images in addition to text, and express the unique personality of an individual business.
- b. Use high-quality materials including wood, metal or non-glossy fabrics.

- c. Limit the number and size of projecting signs.
 - i. Use no more than one projecting sign per entrance.
 - ii. Limit the size of any projecting sign to five square feet.
 - iii. Project signs no more than 36 inches from the building face, and provide at least six inches between the inside edge of the sign and the building.
- d. Relate the design of projecting signs and supports to the character of the building.
 - i. Decorative approaches may be desirable when appropriate to the sign and/or architectural character of the building.
- e. Position projecting signs to complement the building's architectural details.
 - i. Locate signs below the first floor ceiling line, or no more than 14 feet above the sidewalk, whichever is less. Provide at least eight feet from the bottom of projecting signs to the ground in pedestrian areas and 14 feet in areas with vehicular traffic.
- f. Provide sign lighting only with shielded spotlights.
 - i. Utilize high-quality fixtures such as cylinder spots or decorative fixtures. Avoid exposed standard spot and flood light bulbs.
 - ii. Design light supports to complement the design of the sign and building façade.



Projecting sign example.



Projecting sign example.



Quality brackets are encouraged.



Sign variety creates a dynamic streetscape.



Hanging sign.



Hanging sign with high-quality materials.

3.5.9 HANGING SIGNS

- a. Hanging signs are relatively flat panels, generally two-sided, which are similar to projecting signs, but are smaller and suspended below awnings, bay windows, balconies, and similar projections. They are intended primarily for business identification to pedestrians passing on the sidewalk.
- b. Use high-quality materials. Use wood, metal and avoid shiny plastic or fabric. Finish all exposed edges. Suspend signs with metal rods, small scale chains, cable, or hooks.
- c. Limit the number and size of hanging signs.
 - i. Use no more than one hanging sign per entrance.
 - ii. Limit the maximum sign size to three square feet.
 - iii. Mount signs to provide a minimum of eight feet of clearance between the sign and the sidewalk.
- d. Orient hanging signs to pedestrian traffic.
 - i. Mount signs with their orientation perpendicular to the building face so that they will be visible to pedestrians passing on the sidewalk.
 - ii. If multiple hanging signs are placed along a business frontage, they should all be mounted with their bottom edge the same distance above the sidewalk.

3.5.10 PLAQUE SIGNS

- a. Plaque signs are pedestrian-oriented flat panels mounted to wall surfaces near the entry to a business. They include signs that identify a specific business, directory signs for multiple businesses, and menu boxes for restaurants.
- b. Limit the location and size of plaque signs. Locate signs only on wall surfaces adjacent to tenant entries or entry passageways to off-street courtyards. Signs identifying a single business should be limited to an area of four square feet. Directory signs for the identification of multiple second floor or courtyard tenants may be larger, but no more than eight square feet in area.
- c. Use plaque signs for the display of restaurant menus. Menu signs or boxes should not exceed six square feet in area, and should have internal or indirect lighting (e.g., bulbs located in the frame to cast direct light over the menu surface) or direct lighting using decorative fixtures.



Plaque sign example.



Plaque sign example.



Example of ground sign. Image courtesy of SGPA Architects.



Sample of ground sign.

3.5.1.1 GROUND SIGNS (MONUMENT SIGNS)

- a. Ground signs are signs seven feet or less in height that assist motorists in finding businesses along auto-oriented streets. Informational and Directional Ground Signs are signs that assist motorists in safely exiting from moving traffic lanes to on-site uses.
- b. Limit the information on each sign. Ground sign information should generally be limited to the following information:
 - i. Project or primary business identification name and/or logos.
 - ii. Address number (strongly encouraged).
 - iii. Types of uses or names of businesses located interior to the Specific Plan Area.
- c. Locate signs for easy visibility from passing vehicles.
- d. Lighting for ground signs may be by direct spotlight illumination from fixtures mounted either 1) at the top of the sign, or 2) on the ground below the sign.
- e. Fixtures must be shielded to avoid direct view of the bulb.
- f. Ground Signs shall appear to be made of stone, metal, corten steel, and/or other natural materials and complement the character of the project.
- g. All ground signs shall be constructed of matte finish non-reflective materials.



4. CIRCULATION AND STREETScape



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4. CIRCULATION AND STREETScape

This chapter focuses on pedestrian, bicycle, and vehicular circulation. It contains Goals and Policies and recommendations for internal street design as well as Lark Avenue and Los Gatos Boulevard improvements. Street design includes right-of-way dimensions and desired street trees (refer to Landscape Palette provided in Chapter 3), sidewalk and outdoor furnishing characteristics, pedestrian connections, and transit and bicycle facilities.

4.1 CIRCULATION AND STREETScape GOALS AND POLICIES

Goal: *To incorporate multimodal solutions to create a walkable neighborhood while minimizing traffic impacts within and around the North 40 Specific Plan Area.*

Policy C1: Circulation and Connectivity

Provide linkages between the Specific Plan Area and surrounding neighborhoods with enhanced pedestrian and vehicular circulation improvements.

Policy C2: Traffic

Minimize traffic impacts through site design, multimodal opportunities, land uses, the intensity of development, access, and street and intersection improvements.

Policy C3: Park Once Design

Create a pedestrian-friendly “park once” mixed-use district where visitors, residents and employees park their vehicle once and walk to various destinations within the development.

Policy C4: Pedestrian Orientation

Pedestrian-oriented design is a high priority throughout the Specific Plan Area. Make a concerted effort to ensure the human scale and walkability in the

Specific Plan Area through intentional design and delineation of pedestrian walkways and corridors.

Policy C5: Bicycle Planning

Integrate bicycle facilities and amenities throughout the site. Development projects shall include multi-modal paths, physically separated from vehicle roadways running from north to south and providing connectivity to perimeter paths at both the north and south ends.

Policy C6: Complete Streets

Encourage use of “complete street” strategies for Los Gatos Boulevard and Lark Avenue and primary connectors within the Specific Plan Area. “Complete Streets” are roadways designed and operated to enable safe, attractive, and comfortable access and travel for all users, including pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities.

Policy C7: Parking Design

Minimize impacts of parking lots through location, landscaping, buffers and structure design.

Policy C8: Transit

Work with Santa Clara Valley Transportation Authority (VTA) to locate transit stops in central locations accessible to a majority of the Specific Plan Area.

Policy C9: Connectivity

Connect the Specific Plan Area with Downtown, commercial centers, and other employment centers via light rail transit, bicycle paths, or trails.

Policy C10: Streetscape Furnishings

Create a consistent neighborhood identity throughout the Specific Plan Area by coordinating streetscape furnishings such as benches, street lighting, trash and recycling containers, planter pots and signage (refer to Neighborhood Identity section in Chapter 3).



Existing Los Gatos Boulevard looking North.



Existing Los Gatos Boulevard looking North.

4.2 SITE ACCESS

There will be three primary access points located along Los Gatos Boulevard and one on Lark Avenue. By having multiple access points to the Specific Plan Area, the potential for congestion at any one entry point is lessened. The first access point at Burton Road, located off of Los Gatos Boulevard, will be improved and provide a northern access point into the Specific Plan Area, and the existing signal will remain. A second key point of entry will be at Noddin Avenue. The third access point along Los Gatos Boulevard will be approximately three hundred feet south of Noddin Avenue. There is also an opportunity for a fourth access point off of Los Gatos Boulevard closer to the Lark Avenue intersection. Access to the Specific Plan Area will primarily be from these points. Any additional access drives, beyond those that serve the existing businesses today, are discouraged.

The Lark Avenue entrance will be in the vicinity of Highland Oaks Drive a minimum of 300 feet from Los Gatos Boulevard and a minimum of 300 feet from the State Route (Highway 17) on-ramp.

4.3 STREET SYSTEM

The street layout depicted in the Circulation Plan in Figure 4-1, provides a hierarchical system of streets that are purposefully designed to establish an identity, a walkable environment, and efficient connections within the Specific Plan Area and to adjacent neighborhoods. The internal street system consisting of 'A' Street, Neighborhood Street, Noddin Avenue, and Burton Road shall provide through access. Gated streets or limited access, except for special events, is prohibited on these primary streets. Streets shall be designed with a unified and well connected system of tree-lined streets, sidewalks, and paseos that focus on the pedestrian environment.

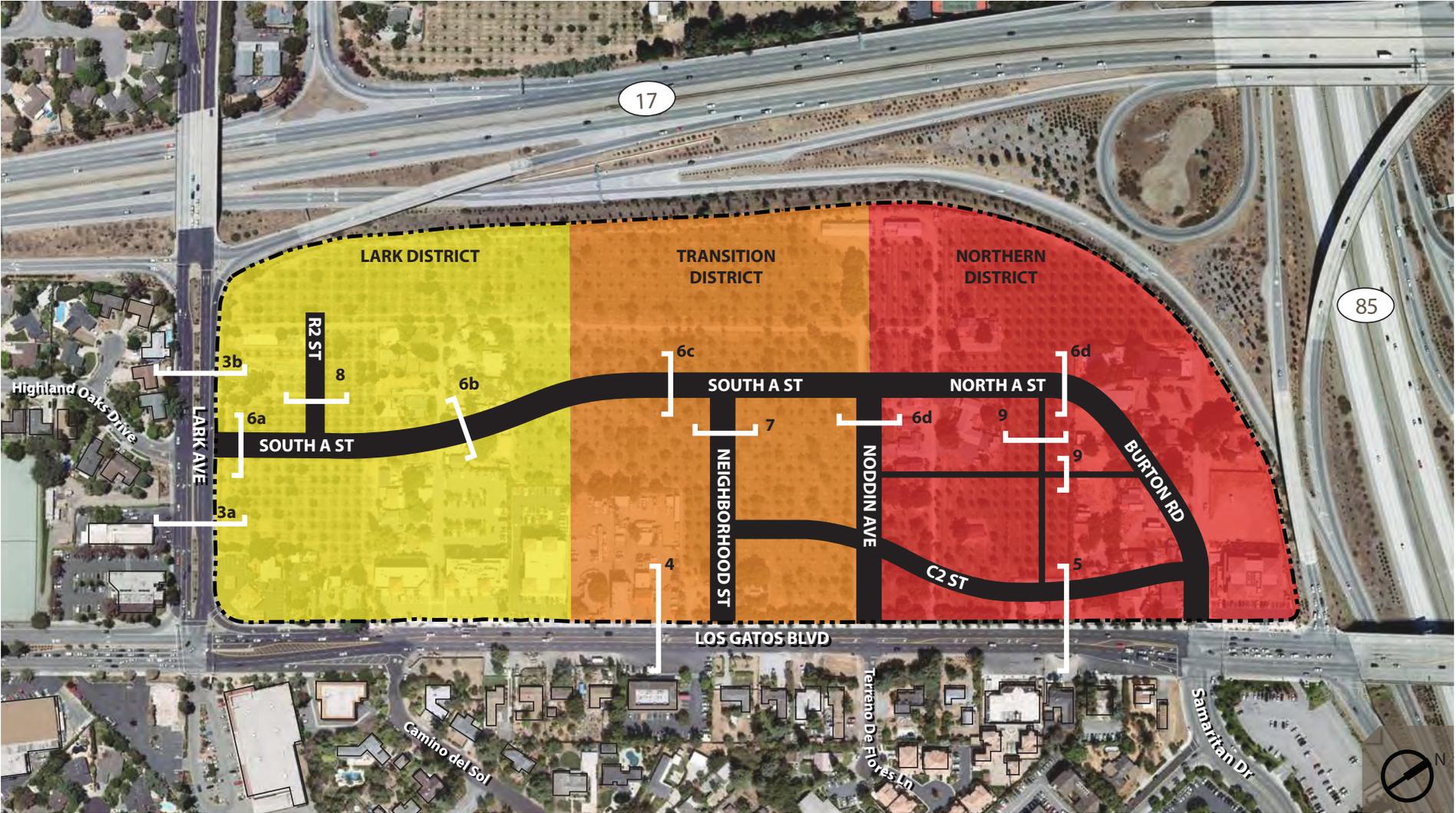


Figure 4-1 Circulation Plan - Cross Sections Identified

Note:
Street locations are conceptual. Exact location shall be included in future Architecture and Site Review application.



Orchard planting scheme incorporated within the streetscape design.



Street trees, wide sidewalks, and street furnishings create a pedestrian friendly streetscape.

4.4 STREET IMPROVEMENTS

Los Gatos Boulevard and Lark Avenue improvements are crucial to establishing a character and identity for the Specific Plan Area. Enhancing these streets with landscaping, landscaped parkways (planting strips) that buffer sidewalks and multimodal paths from travel lanes and incorporating the character and charm of the orchard will create a unique neighborhood identity for the Specific Plan Area and a gateway to Los Gatos. Similar streetscape treatments will be used on streets within the Specific Plan Area, in addition to pedestrian features such as special paving at crosswalks and street furniture to further reinforce an image customized for the area.

4.5 STREETScape DESIGN

Carefully designed and implemented streetscape improvements will enhance the pedestrian experience within the Specific Plan Area and encourage pedestrian activity and movement throughout. Sidewalks will be improved with amenities such as street trees, furnishings, pedestrian scale lighting, curb extensions at intersections and specialized crosswalk paving. Curb extensions, also known as “bulbouts”, at key locations will help minimize pedestrian crossing time and increase safety. Accent paving at crosswalks will emphasize pedestrian ways and add visual appeal as well as help slow automobiles. Special paving can also be employed to distinguish areas within the sidewalk or at building entries.

To create a pedestrian scaled street environment, storefronts in the commercial zones should be sited close to the street to better define the street edge. A continuous storefront experience and attractive walkways will maximize the quality of the pedestrian environment. Some variation in front setbacks is encouraged to allow for outdoor dining opportunities - bringing interest and activity to the street edge.

Streetscape amenities will help define and unify the neighborhood identity along Los Gatos Boulevard, Lark Avenue, and within the Specific Plan Area (refer to Neighborhood Identity section in Chapter 3).

4.6 INTERSECTION IMPROVEMENTS

The following intersection improvement recommendations are important to ensure an efficient flow of traffic on Lark Avenue, Los Gatos Boulevard, through the Specific Plan Area and for access to and from the Specific Plan Area.

The Los Gatos Boulevard/Samaritan Drive/Burton Road intersection should be improved by the first project developer within the Northern District of the Specific Plan Area. The improvements should include:

- Conversion of the existing eastbound lane on Burton Road to a through/left turn lane.
- Addition of one dedicated eastbound left turn lane and one eastbound right turn lane on Burton Road at Los Gatos Boulevard (including widening Burton Road for about 200 feet west from Los Gatos Boulevard).

The Noddin Avenue access point will provide right-in and right-out access to Los Gatos Boulevard and left-in access for turning left from Los Gatos Boulevard to Noddin Avenue and left to Terreno De Flores. A new signalized intersection should be located approximately mid-way between Noddin Avenue and Bennett Way at the intersection of Los Gatos Boulevard/ Neighborhood Street. The following off-site intersection improvements shall be completed at the Los Gatos Boulevard/Lark Avenue intersection by the first project developer:

- Addition of a third eastbound left turn lane on Lark Avenue.
- Addition of third northbound left turn lane on Los Gatos Boulevard.



Intersection improvements can include bulbouts and enhanced paving treatment at crosswalks.



Bulbouts provide pedestrian refuge and increased visibility at street corners while calming traffic.

- Addition of a third westbound lane on Lark Avenue, which will operate as a second right turn lane for the State Route 17 on-ramp.
- Modification and re-stripping of intersection and restriction of parking as needed.

Access to 'A' Street off of Lark Avenue will be provided with a left-in, right-out access to the Specific Plan Area. A dedicated right-in turn lane on Lark Avenue to access "A' Street is optional. A signal is not planned for this intersection.

4.7 TRAFFIC CALMING ELEMENTS

Traffic calming measures are aimed at reducing traffic speeds and increasing pedestrian connectivity. Some traffic calming elements proposed in the Specific Plan Area include bulbouts, narrow street sections, street trees and planting strips, accent paving at crosswalks and intersections, and pervious paving in on-street parking zones.

- Bulbouts should be used to help narrow roads and shorten the pedestrian crossing distances, while improving pedestrian visibility to motorists.
- Canopy trees and planting strips should be used to help frame the street, narrowing the perceived street width, and slowing traffic.
- Enhanced paving in crosswalks, at intersections, mid-block crossings, and on-street parking zones should be used to help signify a change of ground plane and reduce the speed of traffic.

4.8 PEDESTRIAN CIRCULATION

In an effort to implement the Specific Plan's vision of creating a pedestrian-oriented neighborhood environment, it is important to establish well connected pedestrian paths, paseos, and sidewalks throughout the Specific Plan Area. Paseos, paths, and sidewalks should interconnect throughout the site, leading to and through plazas and green spaces. This encourages a vibrant pedestrian environment that invites people to walk and ride their bicycles around the neighborhood. Integrating paseos and plazas into the Northern District will help create a sense of place for the district.

4.8.1 PEDESTRIAN PASEOS AND PATHWAYS

A network of paseos and pedestrian pathways should be interwoven throughout the North 40 Specific Plan Area to connect the districts and neighborhoods and also connect to the streets and surrounding Perimeter Overlay Zone.

Vegetated pathways within the Lark District should be designed to weave between the residential neighborhoods connecting common areas, pocket parks, and streets.

The paseos and pathways within the Transition District should connect courtyards, plazas, and streets to the neighborhood commercial areas and to the Lark District neighborhood.

Within the Northern District, a series of paseos should meander through the commercial core, creating a comfortable pedestrian experience. Paseos will provide many opportunities for outdoor dining and storefront shopping. A paseo works as a pedestrian street, furnished with benches, tables, street trees, planters, and may include focal features, fountains, and public art.



Pedestrian pathway example (Lark District).



Figure 4-2 Typical Pathway (Lark District)



Paseos provides a vibrant pedestrian environment throughout the Specific Plan Area.



Pedestrian paseo example (Transition and Northern Districts).



Figure 4-3 Section 9: Typical Paseo (Transition and Northern Districts)

4.9 BICYCLE TRAVEL

Bicycle facilities will be integrated throughout the Specific Plan Area. Bicycle travel promotes a healthy alternative to vehicle transportation. Bicycle facilities include: Class II bicycle lanes, multimodal paths, sharrow, bicycle racks, and bicycle lockers. Class II bicycle lanes are striped lanes within the road right-of-way. Multimodal paths are separated from roadway and designated for two way pedestrians and bicycle travel. Sharrow refers to shared lane pavement markings placed within a travel lane to indicate that a bicyclist may use the full lane. The sharrow symbol consists of a bicycle symbol with two chevron markings above the bicycle (refer to right image).

- It is anticipated that 'A' Street within the Specific Plan Area will include sharrow.
- Require all bike lanes and sharrow to be painted green.
- A north-south multimodal path should be provided to facilitate connectivity within the Specific Plan Area.

4.10 TRANSIT DEMAND MANAGEMENT

The Specific Plan Area is currently located outside of an established core or transit station area which may make it challenging to attract a significant share of residents, employees and visitors to use transit. Therefore, Transportation Demand Management (TDM) programs are required for each application within the Specific Plan Area. Effective TDM programs can help address both the roadway congestion and Vehicle-Miles-Traveled (VMT) generated by the development within the area and may include:

- Parking cash-out.
- Financial incentives for taking alternative modes.
- Transit Fare incentives such as Eco Pass and Commuter Checks.
- Preferentially located carpool parking.



Sharrow provide a shared use of the travel lane.



VTA provides service to the Town of Los Gatos.



Parking areas are buffered from view by planters and trees.

- Bicycle lockers and bicycle racks.
- Showers and clothes lockers for bicycle commuters.
- On-site or walk-accessible employee services (day-care, dry-cleaning, fitness, banking, convenience store).
- On-site and off-site shuttle services.

4.11 TRANSIT STRATEGIES

Los Gatos is currently served by the Santa Clara Valley Transportation Authority (VTA) bus service. There are currently two bus stops along Los Gatos Boulevard adjacent to the Specific Plan Area. Other bus connections can be made across the street from Burton Road on Samaritan Drive within the Good Samaritan Hospital area. There is an opportunity to incorporate additional bus stops adjacent to or within the Specific Plan Area. These stops could also provide a future bus connection to the Vasona Light Rail facility.

In an effort to create a strong connection between the Specific Plan Area and Downtown Los Gatos, a trolley service could be provided that loops within the area and connects to Downtown and other transit services. This type of service would further encourage the “park once, shop twice” ideal and enhance the pedestrian-oriented environment envisioned for the Specific Plan Area.

4.12 PARKING STRATEGIES

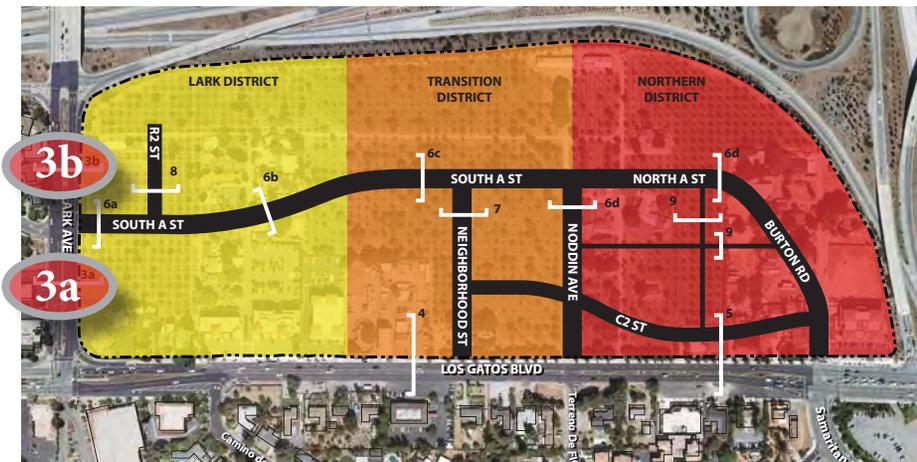
The following parking design and location strategies should be implemented within the Specific Plan Area to reduce the dominance of automobiles and help create a safe and aesthetically pleasing pedestrian environment.

- Parking areas should be sufficiently buffered from view and tucked underneath residential structures and behind retail and mixed-use development.

- Diagonal parking, 90 degree parking, and/or parallel parking should be provided along the street edge fronting commercial uses on internal streets.
- Shared parking and reciprocal access to parking lots is strongly encouraged between adjacent developments and businesses.
- Parking structures should be considered to help minimize the land area dedicated to surface parking.

4.13 SPECIFIC PLAN AREA CIRCULATION IMPROVEMENTS

The following sections depict potential road configurations in the Specific Plan Area and future improvements to Lark Avenue and Los Gatos Boulevard. Further detail can be found in the EIR Transportation Impact Analysis.



Key Map

4.13.1 LARK AVENUE

It is anticipated that Lark Avenue will be improved by expanding the right-of-way into the Specific Plan Area and incorporating a planting strip, multimodal path, and an orchard themed setback. Pedestrian connections will be strengthened to surrounding neighborhoods with improved crosswalks at intersections and the multimodal path. Buildings within the Specific Plan Area will be set back to create an orchard character along Lark Avenue (refer to Table 2-5 Primary Frontage Setbacks).

Between Los Gatos Boulevard and South 'A' Street on Lark Avenue, westbound Lark Avenue will consist of three travel lanes and one optional right turn lane into South 'A' Street. West of 'A' Street (between 'A' Street and Highway 17), Lark Avenue will have one dedicated right turn lane to northbound Highway 17 on-ramp, one through-right turn lane accessing the Highway 17 on-ramp and allowing through traffic over Highway 17 bridge, and one through lane over the bridge. There is a 10 foot right-of-way dedication on the north side of Lark Avenue. Heading east on Lark Avenue there is a dedicated left turn lane to access South 'A' Street.

A multimodal path designed to Caltrans standards and will be the designated bike connection along Lark Avenue. The right-of-way is sized to accommodate future bicycle lanes by reducing size of median and lane widths.

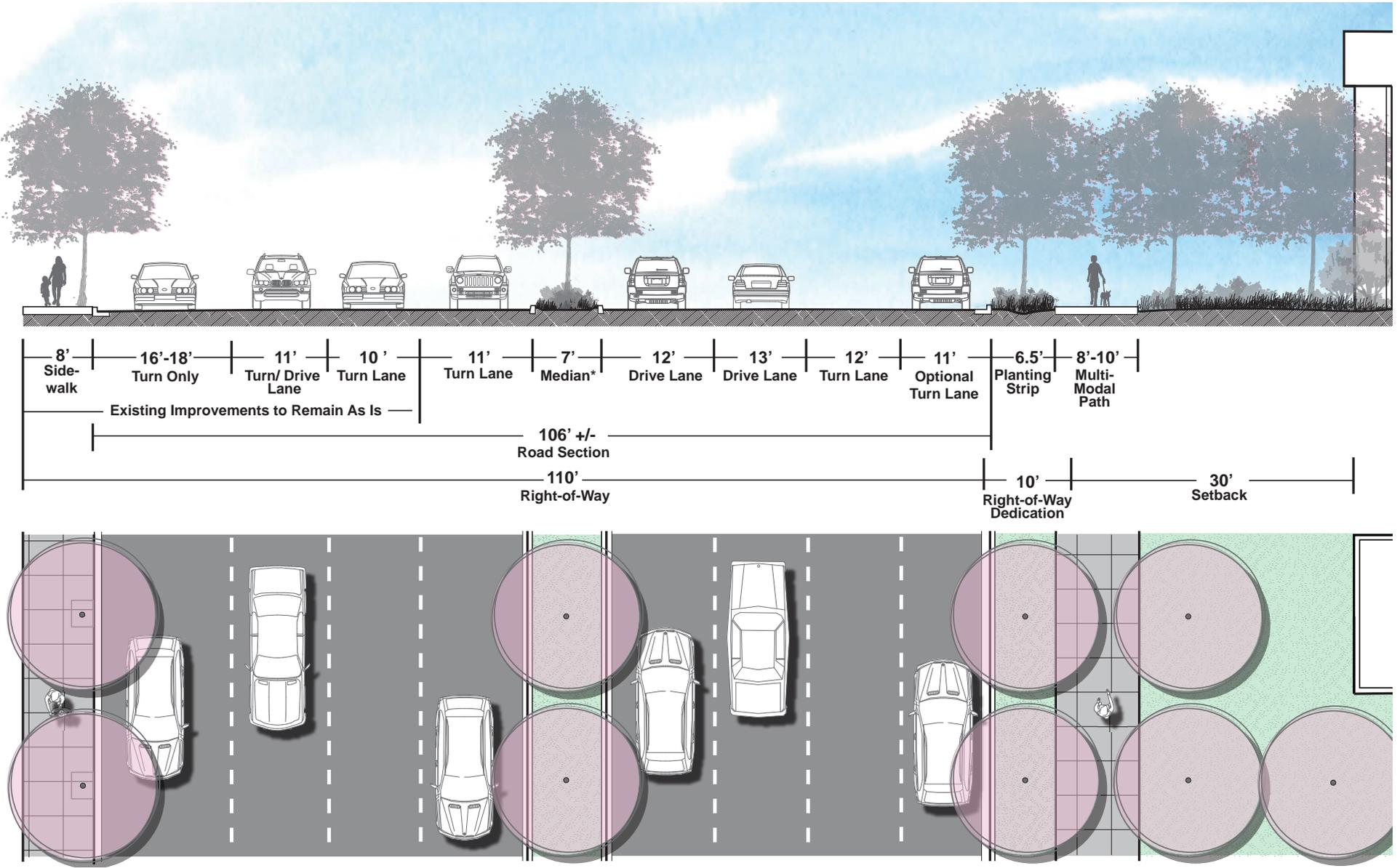


Figure 4-4 Section 3a: Lark Avenue looking west between Los Gatos Boulevard and South 'A' Street. Note: If multimodal path extends outside of right-of-way it will require an easement. * Right-of-way is sized to accommodate future bike lanes by reducing size of median and/or lane widths.

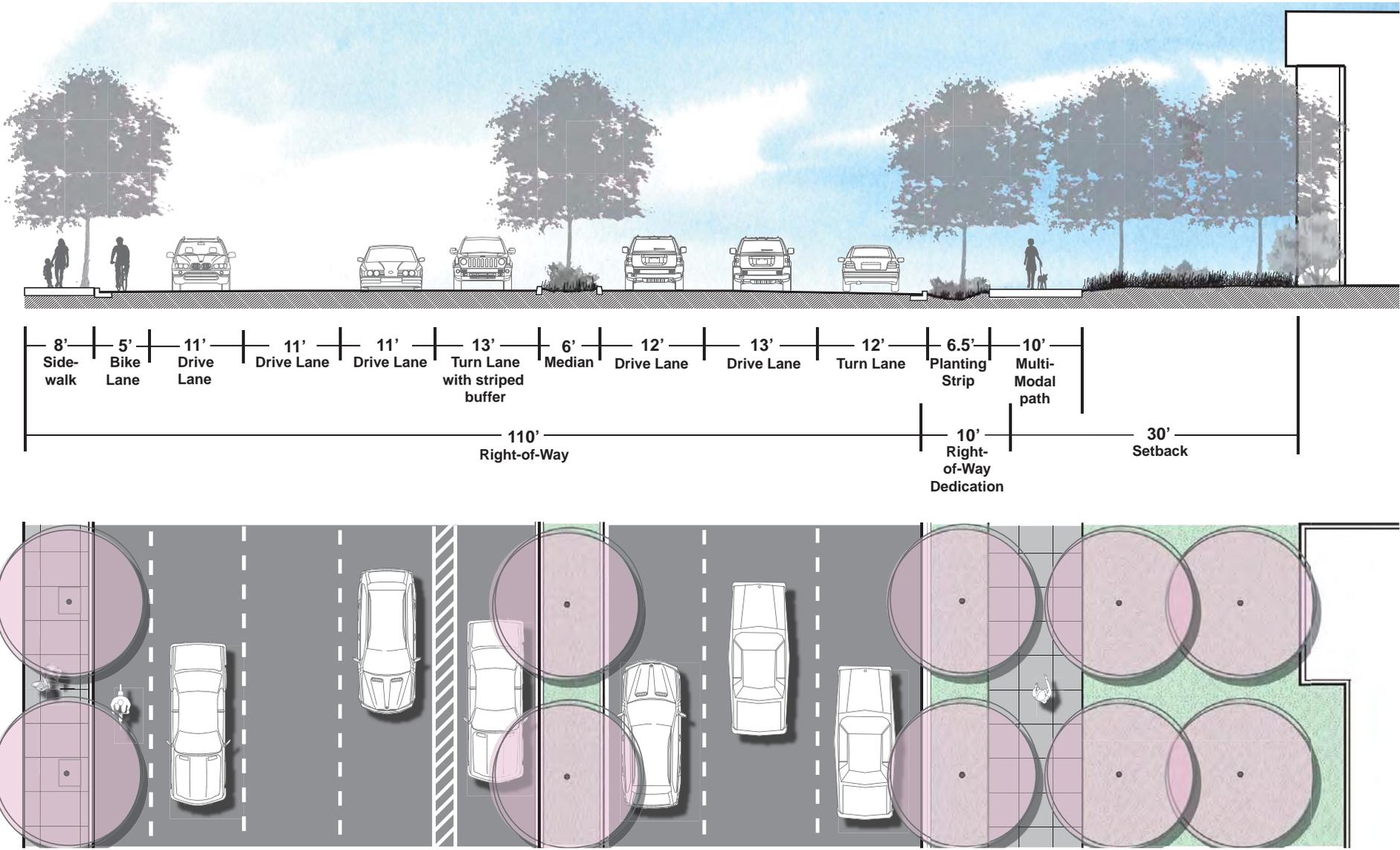


Figure 4-5 Section 3b: Lark Avenue looking west between 'South A' Street and Highway 17. Note: Sidewalks/multimodal paths which extend outside of right-of-way will require an easement.

4.13.2 LOS GATOS BOULEVARD

Los Gatos Boulevard will transition through various alignments between Lark Avenue and Burton Road with two travel lanes in each direction. Generous building setbacks will provide room for orchard planting and other landscaping along Los Gatos Boulevard (refer to Chapter 2 setbacks section 2.5.6).

The existing traffic signals will remain at Burton Road and Lark Avenue intersections and a new signal is proposed at Neighborhood Street. There will be a minimum of one (for first phase of development) and up to two

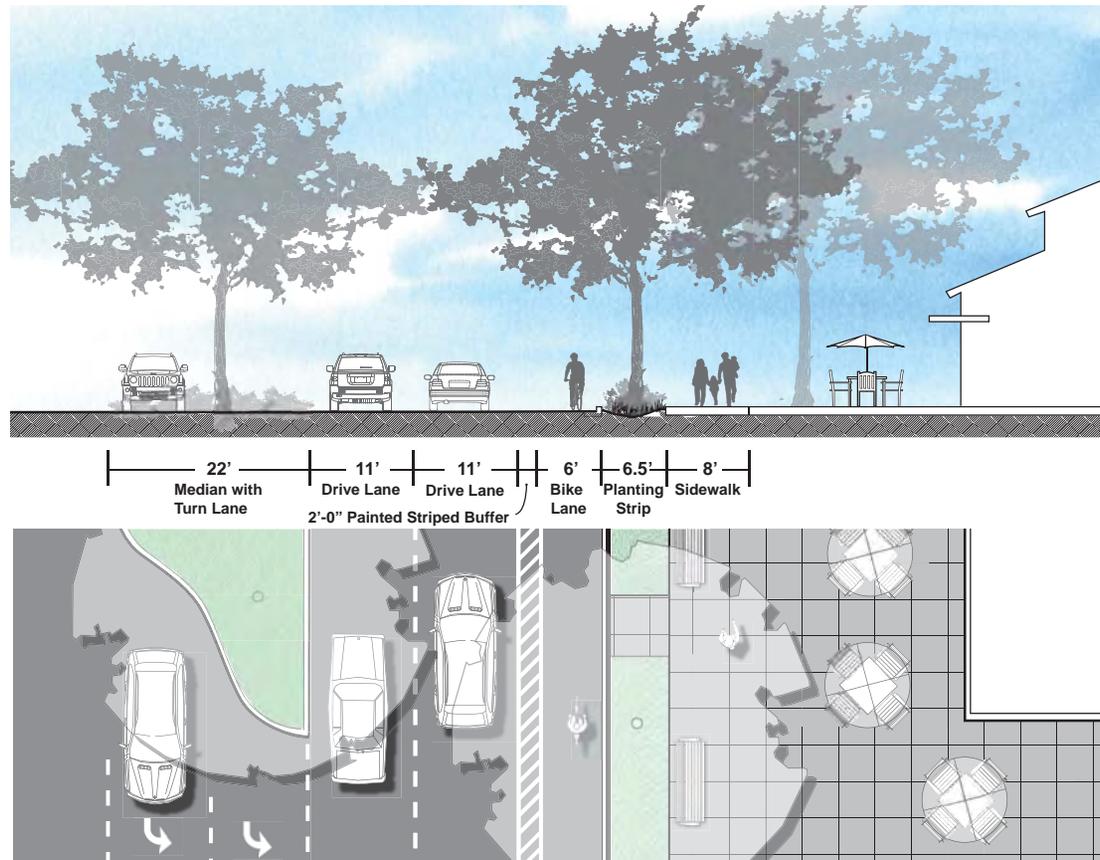


Figure 4-6 Section 4: Los Gatos Boulevard south of Neighborhood Street future alignment. Note: Sidewalks which extend outside of right-of-way will require an easement.

left turn lanes (at full build-out) on northbound Los Gatos Boulevard turning into Neighborhood Street. The Specific Plan outlines two solutions for accommodating bike travel on Los Gatos Boulevard:

- 1) a future alignment with bike lanes on either side of Los Gatos Boulevard, and
- 2) an interim alignment that accommodates two-way bike travel on a multimodal path on the west side of Los Gatos Boulevard on sections that cannot accommodate the bike lanes due to existing width restrictions on the east side of Los Gatos Boulevard.

Los Gatos Boulevard north of Neighborhood Street will maintain two travel lanes with a planting strip/tree grates and separated sidewalk. An expansive setback will allow for orchard tree planting and/or other landscaping along Los Gatos Boulevard.

A center median would be constructed on Los Gatos Boulevard from Samaritan Drive to Lark Avenue, which would prevent left turns at Terreno de Flores Lane, Camino del Sol, Noddin Lane and Bennett Way. Left turns into the Plan Area from Los Gatos Boulevard would be allowed at the existing Samaritan Drive/Burton Road intersection and at a new signalized intersection at Neighborhood Street.

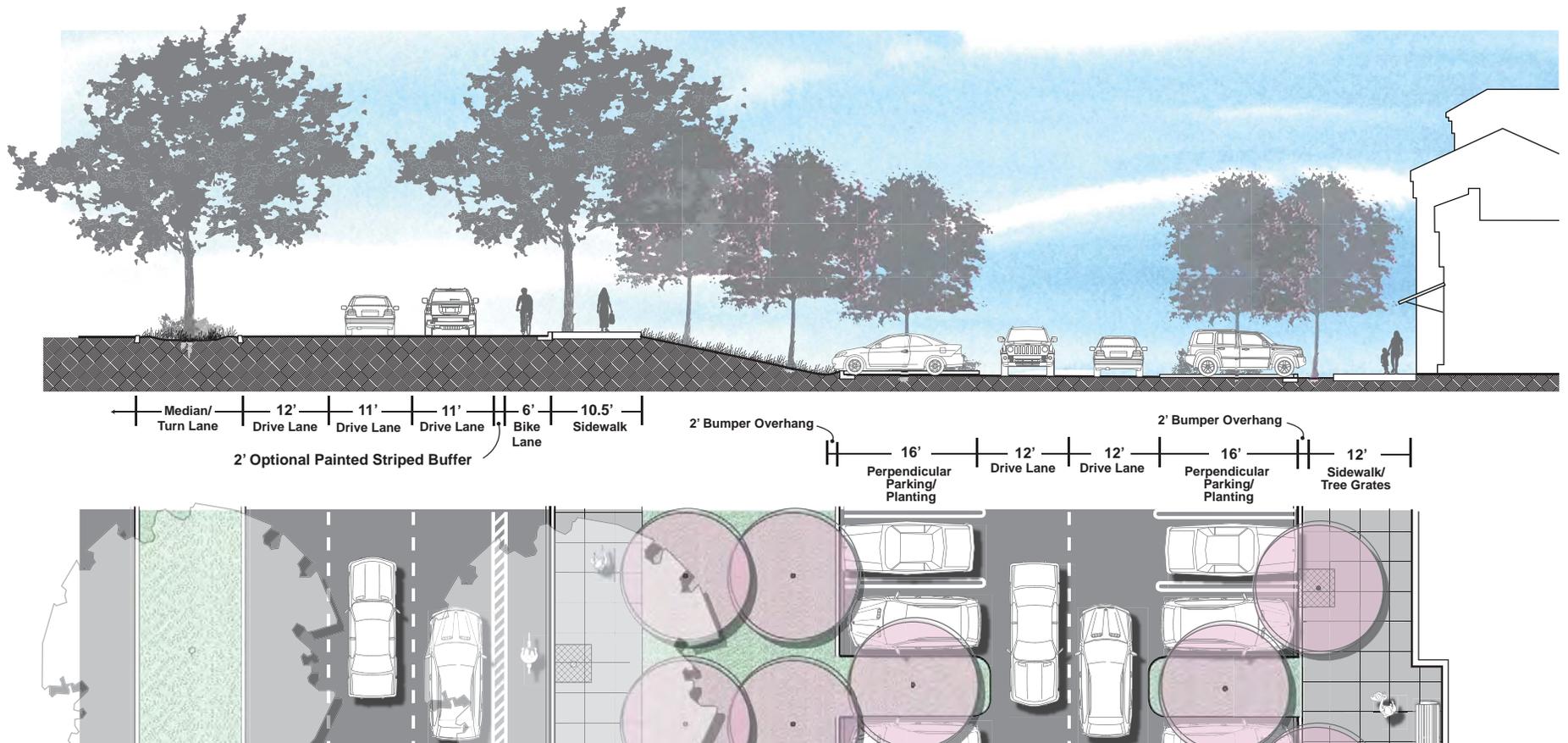


Figure 4-7 Section 5: Los Gatos Boulevard north of Neighborhood Street. Note: Sidewalks which extend outside of right-of-way will require an easement.

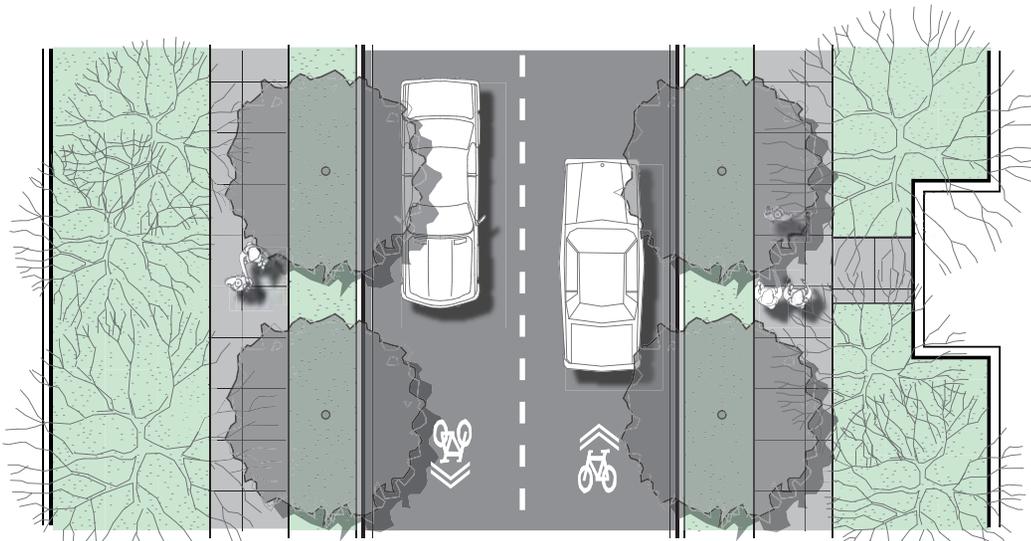
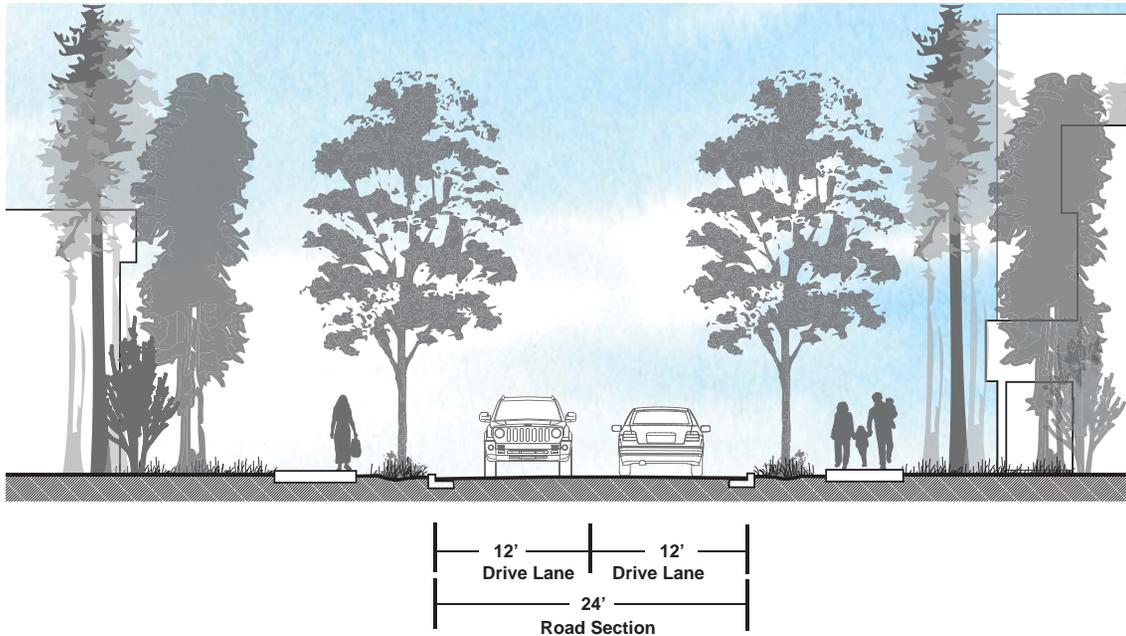


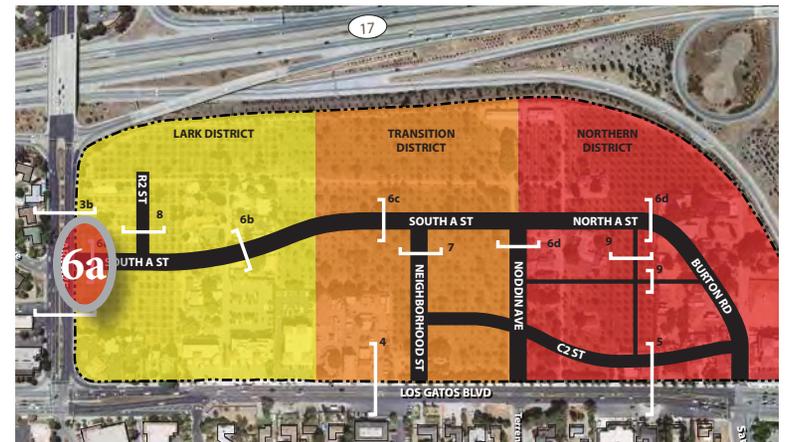
Figure 4-8 Section 6a: South 'A' Street looking south

4.13.3 'A' STREET

'A' Street is the primary connector from Lark Avenue through the Specific Plan Area into the Transition District and Northern District. South 'A' Street will reflect a neighborhood character along the residential portion.

Section 6a:

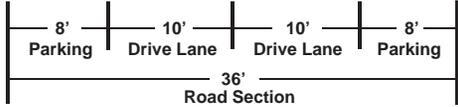
South 'A' Street will provide access to private roads and connect to the mixed-use and commercial portions of the Specific Plan Area. Near the Lark Avenue entrance, South 'A' Street will have drive lanes in both directions with no parallel parking. Sharrowes are proposed to encourage bicycle travel along 'A' Street. Sidewalks and street trees will line both sides of the street.



Key Map

Section 6b:

As South 'A' Street nears the Transition District, the street widens with parallel parking along both sides of the street. Street trees and sidewalks will remain similar on the south end of the street. Sharrows are proposed to encourage bicycle travel along 'A' Street.



Key Map

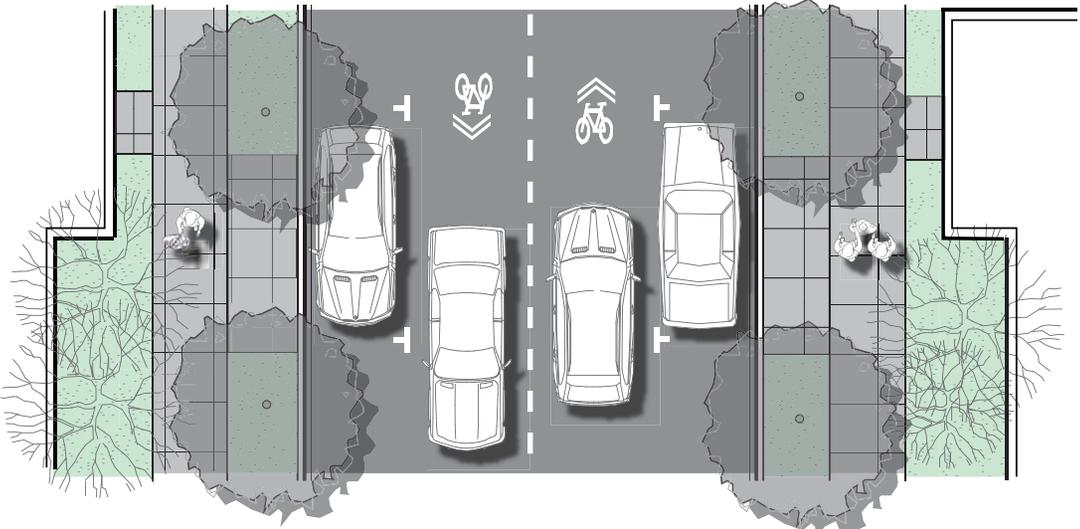


Figure 4-9 Section 6b: South 'A' Street looking south in the Lark District

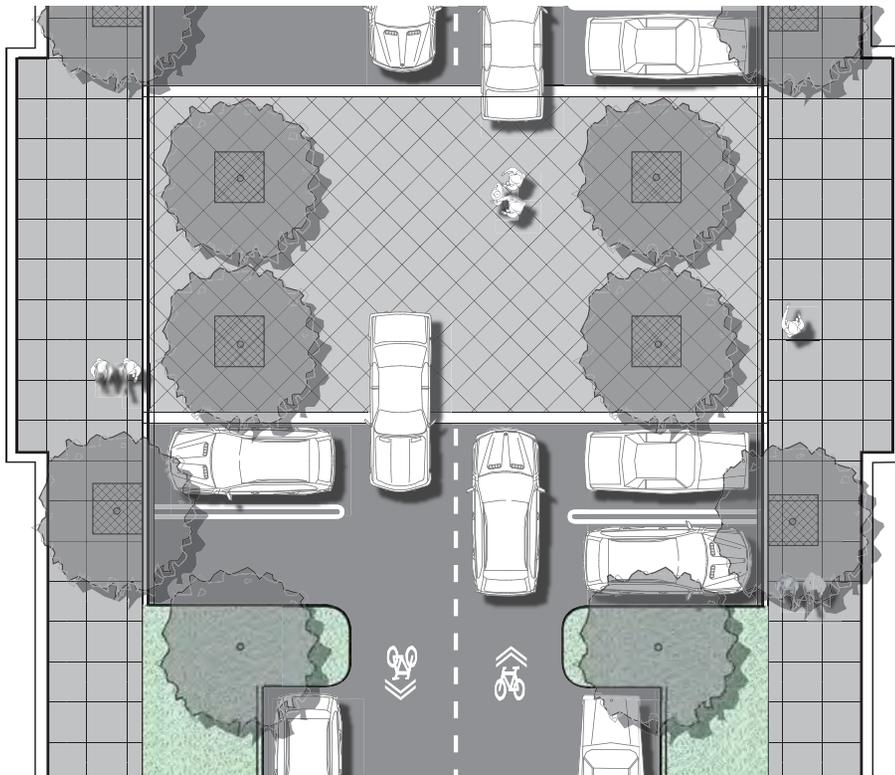
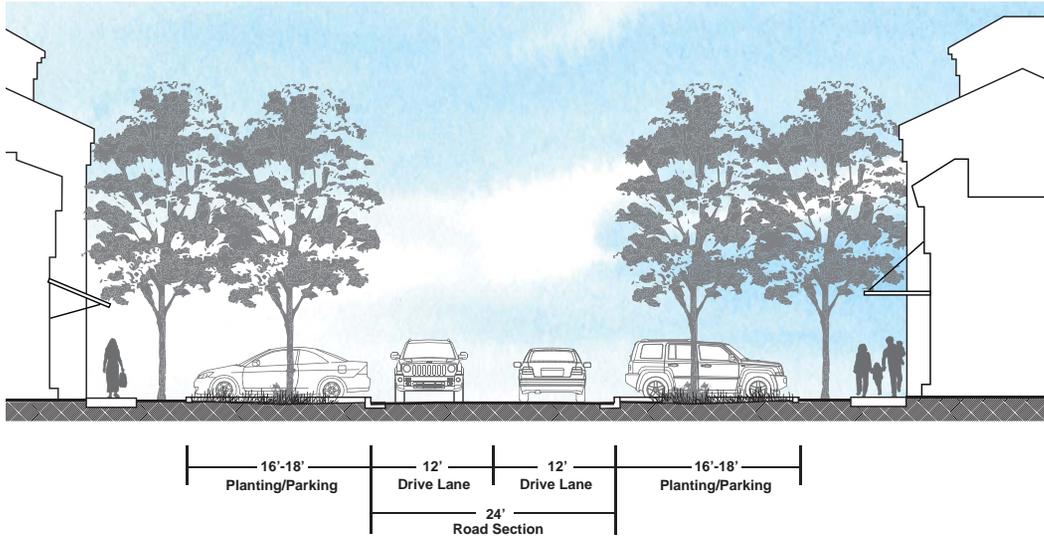
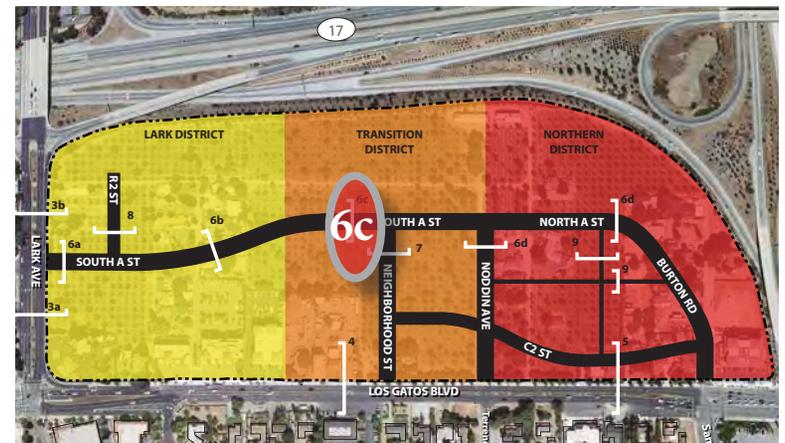


Figure 4-10 Section 6c: South 'A' Street looking north in the Transition District

Section 6c:

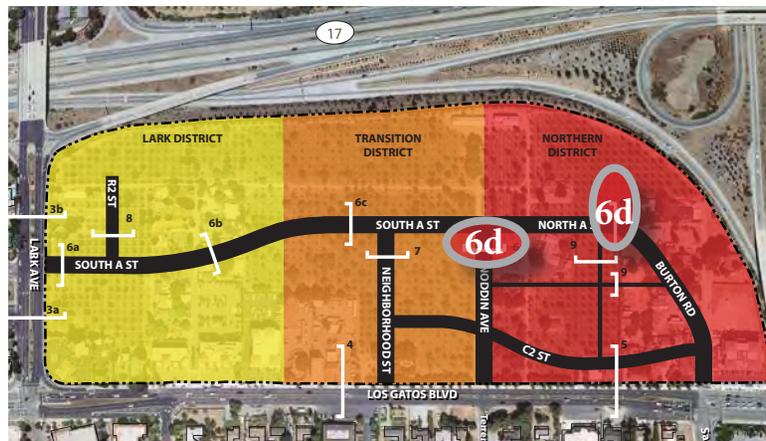
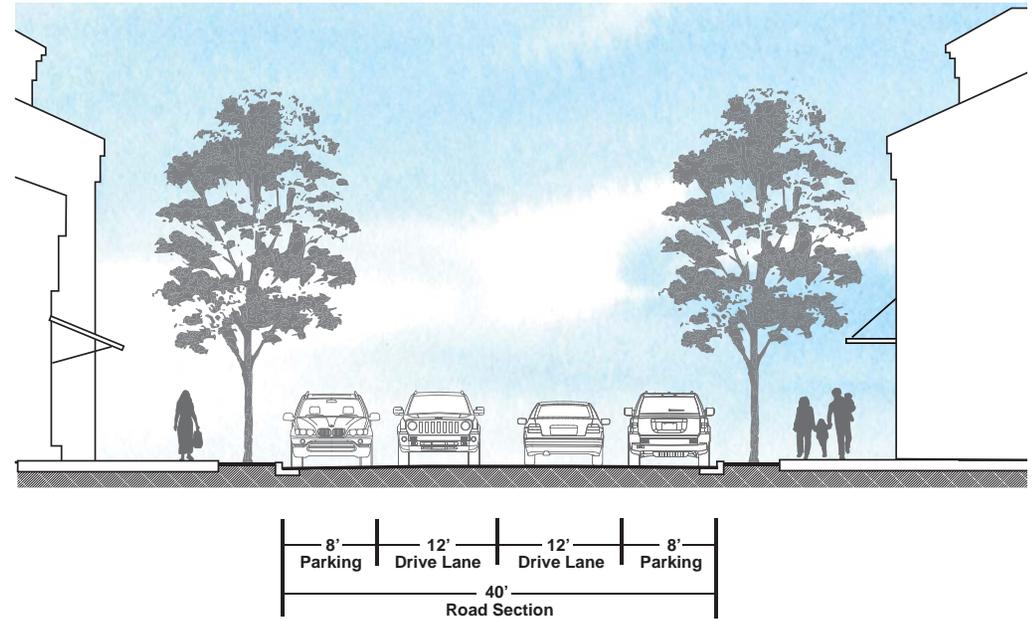
As 'A' Street transitions into the Transition District and Northern District, the character of the street will change, emphasizing the pedestrian-oriented environment. Drive lanes will widen with either parallel parking or pockets of perpendicular parking providing a buffer to the pedestrian environment. Sidewalks will increase to ten feet and street trees will be planted within tree grates to maximize the pedestrian environment. Pedestrian crosswalks with special paving may be incorporated to encourage vehicles to slow down and provide pedestrians access across the street as integral nodes within the Specific Plan Area. Sharrows are proposed to encourage bicycle travel along 'A' Street.



Key Map

Section 6d:

North 'A' Street, Noddin Avenue, and Burton Road create a traditional village experience with wider sidewalks and storefront shopping. 'A' Street will be the primary pedestrian corridor connecting north and south neighborhoods, and it will contain the largest sidewalks within the Specific Plan Area with trees planted within tree grates to maximize the pedestrian environment. Sharrows are proposed to encourage bicycle travel along 'A' Street.



Key Map

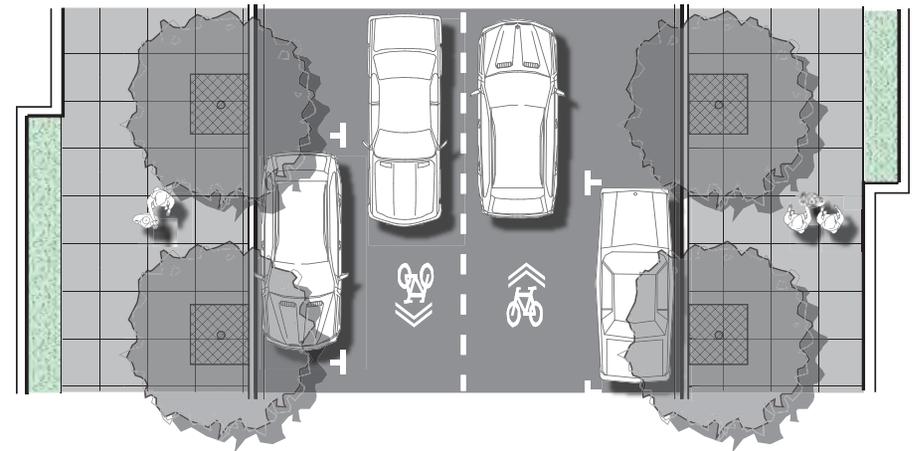


Figure 4-11 Section 6d: North 'A' Street looking north in the Northern District, Noddin Avenue, and Burton Road

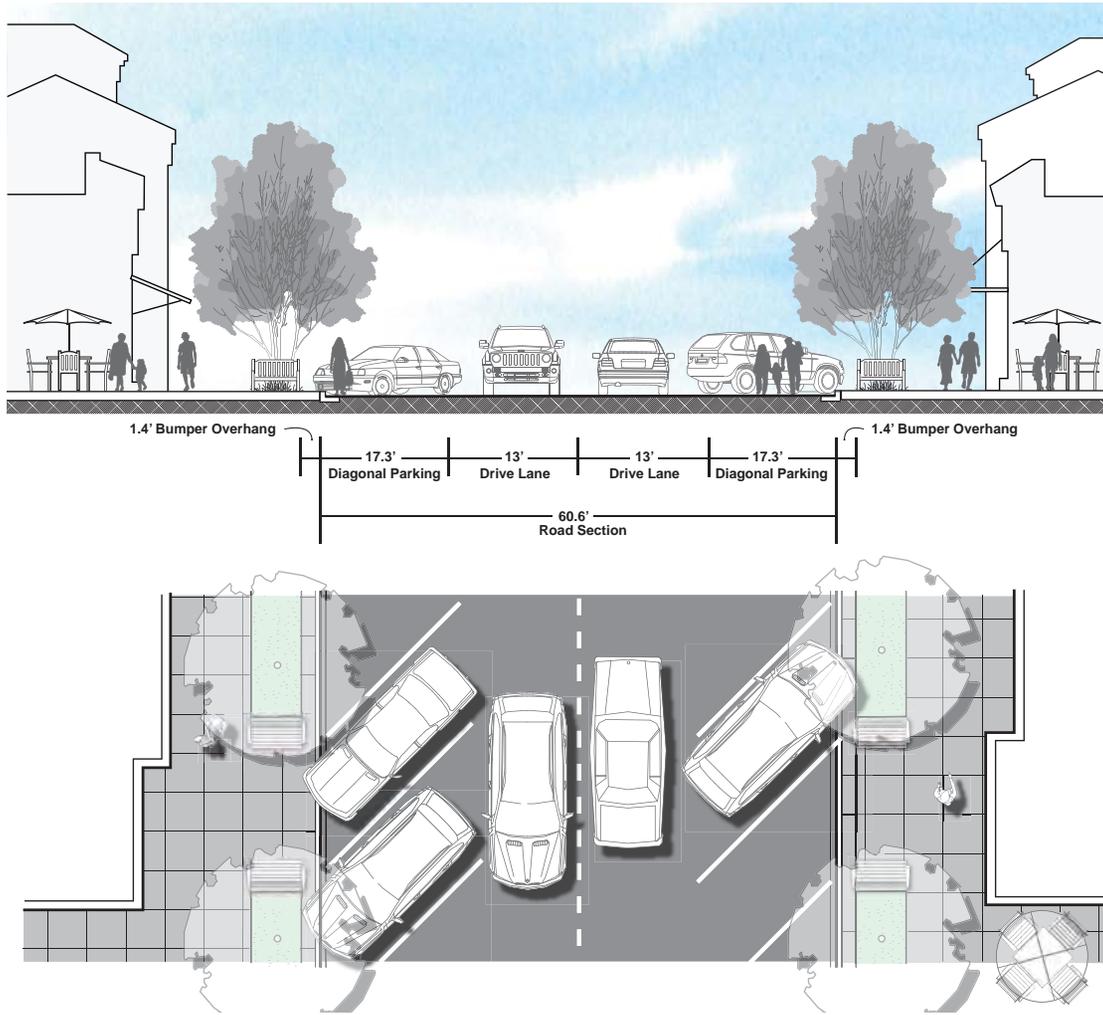
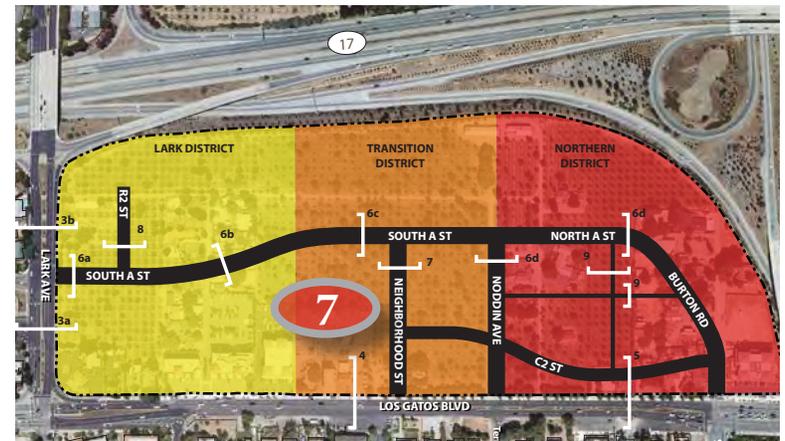


Figure 4-12 Section 7: Neighborhood Street

4.13.4 NEIGHBORHOOD STREET

Neighborhood Street is the primary entrance to the Transition District and will resemble a typical pedestrian-oriented neighborhood commercial street. Diagonal parking will be provided on both sides of the two-lane roadway and wide sidewalks, with integrated planters and seating areas, will provide a connection to neighborhood serving storefronts and outdoor dining. Neighborhood Street near Los Gatos Boulevard will have four lanes with two eastbound lanes approaching Los Gatos Boulevard (one left turn and one right turn lanes) and two westbound lanes (one continuing through and one for turning right into the first parking lot).

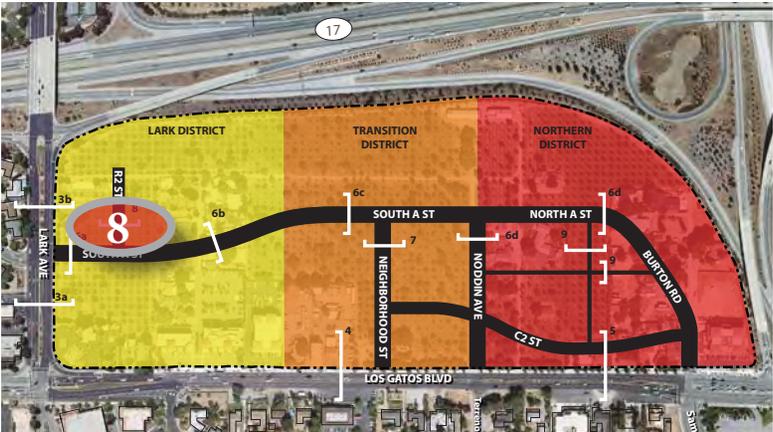
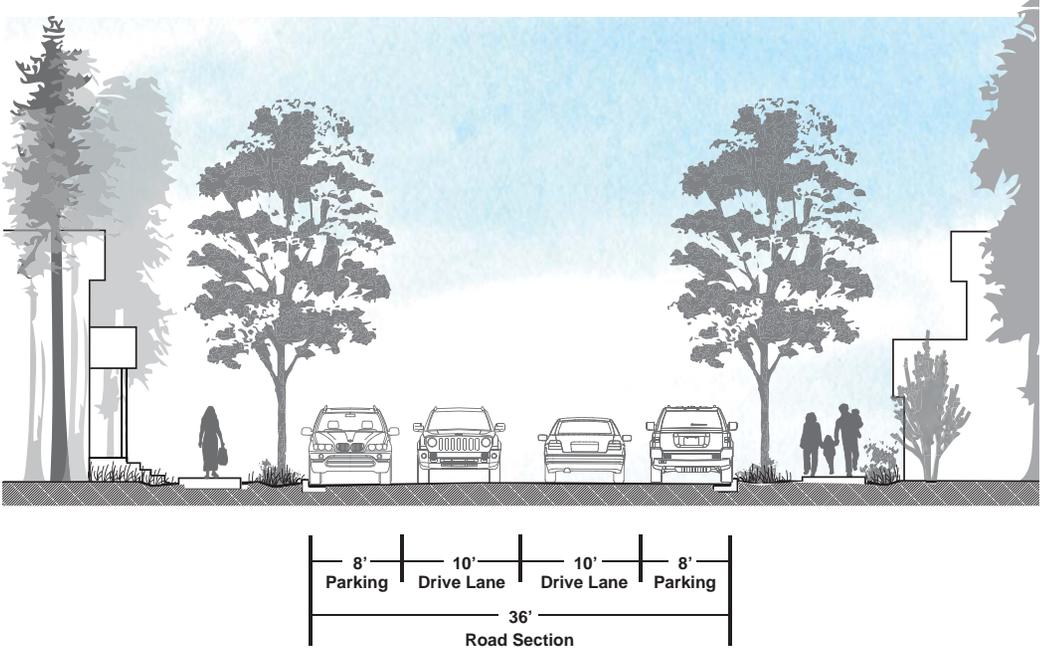


Key Map

4.13.5 INTERIOR RESIDENTIAL STREETS (R2)

Interior streets located in the Lark District have a neighborhood street character with large street trees and sidewalks. To offer a variety of street configurations, this Specific Plan defines two alternatives.

Alternative A provides a typical private road with parallel parking on both sides of the street, planting strips separating the sidewalk from the street, and street trees lining both sides of the street.



Key Map

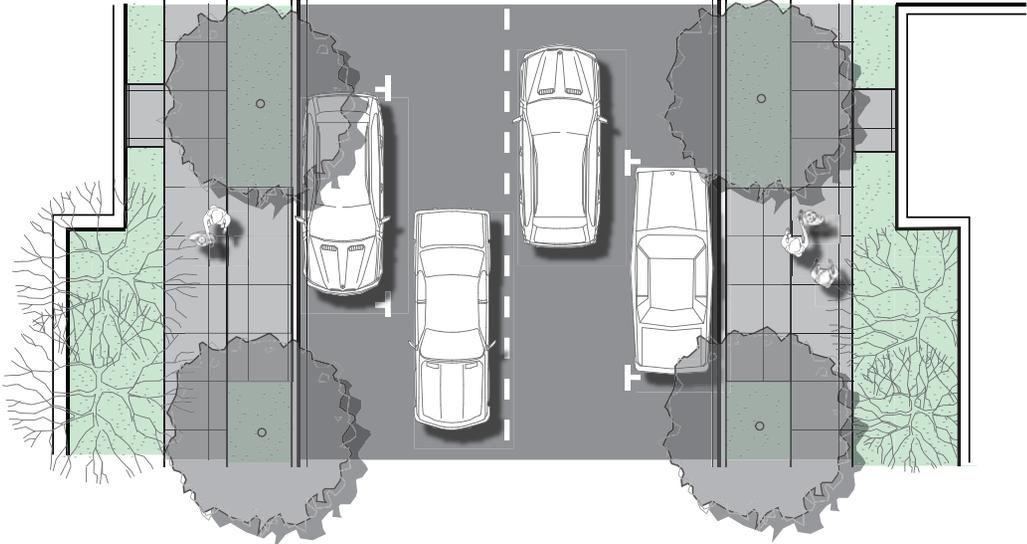


Figure 4-13 Section 8: R2 Street Alternative A

4 CIRCULATION AND STREETScape



Alternative B minimizes emphasis of paving and creates a more intimate neighborhood feel. The Alternative B streets are narrow with no on-street parking. Parking will be provided on 'A' Street, off alleys, and in garages. An option for Alternative B would be to provide parking on one side of the street.

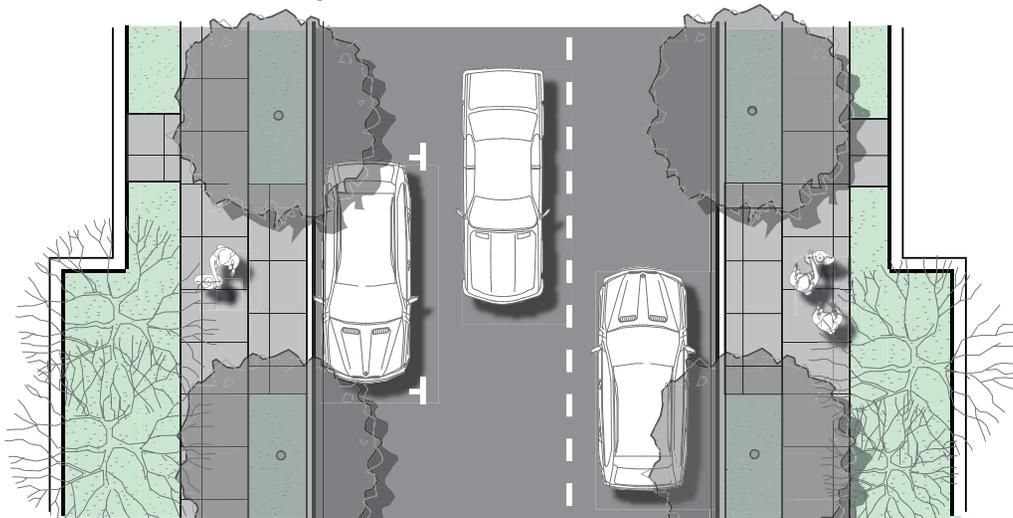
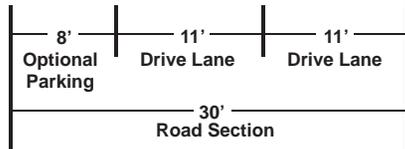


Figure 4-14 Section 8: R2 Street Alternative B



5. INFRASTRUCTURE AND PUBLIC FACILITIES

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5. INFRASTRUCTURE AND PUBLIC FACILITIES

5.1 INTRODUCTION

The construction of on-site and off-site infrastructure improvements will be required to serve proposed development within the Specific Plan Area. The Specific Plan is intended to plan for infrastructure and services that meet Town and other utility agency standards, without diminishing services to existing residents or businesses.

This chapter provides an overview of the major utility infrastructure improvements and the public facilities needed to serve full build-out of the Specific Plan Area. Utilities addressed include stormwater, potable water, wastewater, electricity, natural gas and telecommunications. Services include law enforcement, fire protection, waste management, parks and open space, and public schools. Transportation infrastructure requirements are addressed in Chapter 4, Circulation and Streetscape. Phasing and financing are addressed in Chapter 6.

5.2 INFRASTRUCTURE AND PUBLIC FACILITIES GOALS AND POLICIES

Goal: *To meet the infrastructure and public facilities needs of the Specific Plan Area without impacting existing developed areas.*

Policy I1: Sustainable Solutions

Minimizing impacts to the environment and maximizing sustainability.

Policy I2: Phasing

Developing cost effective solutions that can be constructed in phases.

Policy I3: Flexible Design Solutions

Providing flexible options that can adapt to market conditions.

Policy I4: Water

Provide water-saving solutions in new developments and utilize water-efficient irrigation management systems and devices, such as evapotranspiration or soil moisture-based irrigation controls.

Policy I5: Stormwater

Limit the development of impervious surfaces, to the extent practical in order to reduce post-project runoff rates and promote harvesting rainwater and infiltration, to the greatest extent practical, for irrigation and/or other non-potable purposes.

Policy I6: Construction

Construction and grading activities shall comply with Best Management Practices and Stormwater Pollution Prevention Plan policies per Regional Water Quality Control Board requirements. Development projects shall obtain a Construction General Permit that is in compliance with the National Pollutant Discharge Elimination System (NPDES) requirements as well as the requirements for Storm Water Discharges Associated with Construction and Land Disturbances Activities.

Policy I7: Wastewater

Minimize wastewater flows through indoor water use efficiency efforts.

Policy I8: Address School Needs

Developers are encouraged to collaborate with School Districts to address school needs.

Policy I9: School Facilities

Developers are encouraged work closely with School Districts to project enrollment growth and address overcrowding by assisting with identifying

strategies for providing needed school facilities and associated sources of funding.

Policy I10: Public Services and Facilities

Provide adequate and reliable services to residents and businesses within the Specific Plan Area and surrounding neighborhoods.

5.3 STORMWATER

5.3.1 BACKGROUND AND EXISTING CONDITIONS

The Town of Los Gatos (Town), the California Department of Transportation (Caltrans), and the Santa Clara Valley Water District (SCVWD) provide stormwater and flood control protection services within the vicinity of the Specific Plan Area. The Town owns, operates, and maintains the local public storm drain systems within Los Gatos Boulevard and Lark Avenue. Caltrans owns, operates and maintains the public storm drain systems within the State Route 17 and 85 (Highway 17 and 85) rights-of-way. SCVWD operates and maintains regional flood control facilities in the vicinity including Los Gatos Creek.

The Town administers local stormwater quality protection through the San Francisco Bay Region Municipal Regional Stormwater National Pollutant Discharge Elimination System (NPDES) permit. The permit, commonly referred to as the Municipal Regional Permit (MRP), regulates stormwater discharge into the San Francisco Bay. The MRP was issued by the Regional Water Quality Control Board (RWQCB) to several Bay Area jurisdictions and entities including the Santa Clara Valley Urban Runoff Pollution Prevention Program (SCVURPPP). The Town is one of thirteen member agencies (co-permittees) under SCVURPPP.

The Specific Plan Area is located within sub watershed Area 'D' of the Santa Clara Basin "Guadalupe" watershed. The Guadalupe watershed is 171 square miles (109,440 acres). The Area 'D' sub watershed is 2.17 square miles (1,389 acres). The Specific Plan Area drainage shed is approximately 42 acres, or 3%, and is therefore a relatively small portion of the larger regional watershed.

Los Gatos Creek, the discharge location for the Specific Plan Area, is one of several tributaries within the Guadalupe watershed. Los Gatos Creek flows north through the Town of Los Gatos and discharges into the Guadalupe River approximately seven miles north of the Specific Plan Area. The Guadalupe River discharges into the South San Francisco Bay through the Alviso Slough. The total distance between the Specific Plan Area discharge location into Los Gatos Creek and the South San Francisco Bay is approximately 15 ½ miles.

Historic drainage conditions for the Specific Plan Area have been altered over time due to the construction of Highway 17 and 85. As part of the Highway 17/85 interchange construction Caltrans installed segments of a drainage system to collect runoff from the future developed condition of the Specific Plan Area.

The drainage system installed by Caltrans includes a 36 inch storm drain pipe under Highway 17 and a 42 inch outfall into Los Gatos Creek specifically for and directly west of the Specific Plan Area. In anticipation of development of the Specific Plan Area, the Town approved separate construction plans to connect the pipe under the highway to the outfall. As of the time this Specific Plan was prepared and approved only a portion of the 42 inch pipeline was constructed. A gap in the pipe system of approximately 780 feet exists between Oka Road and the outfall.

Stormwater runoff from within the Specific Plan Area currently discharges into three separate collection systems that all eventually discharge into Los Gatos Creek. Existing developed parcels fronting

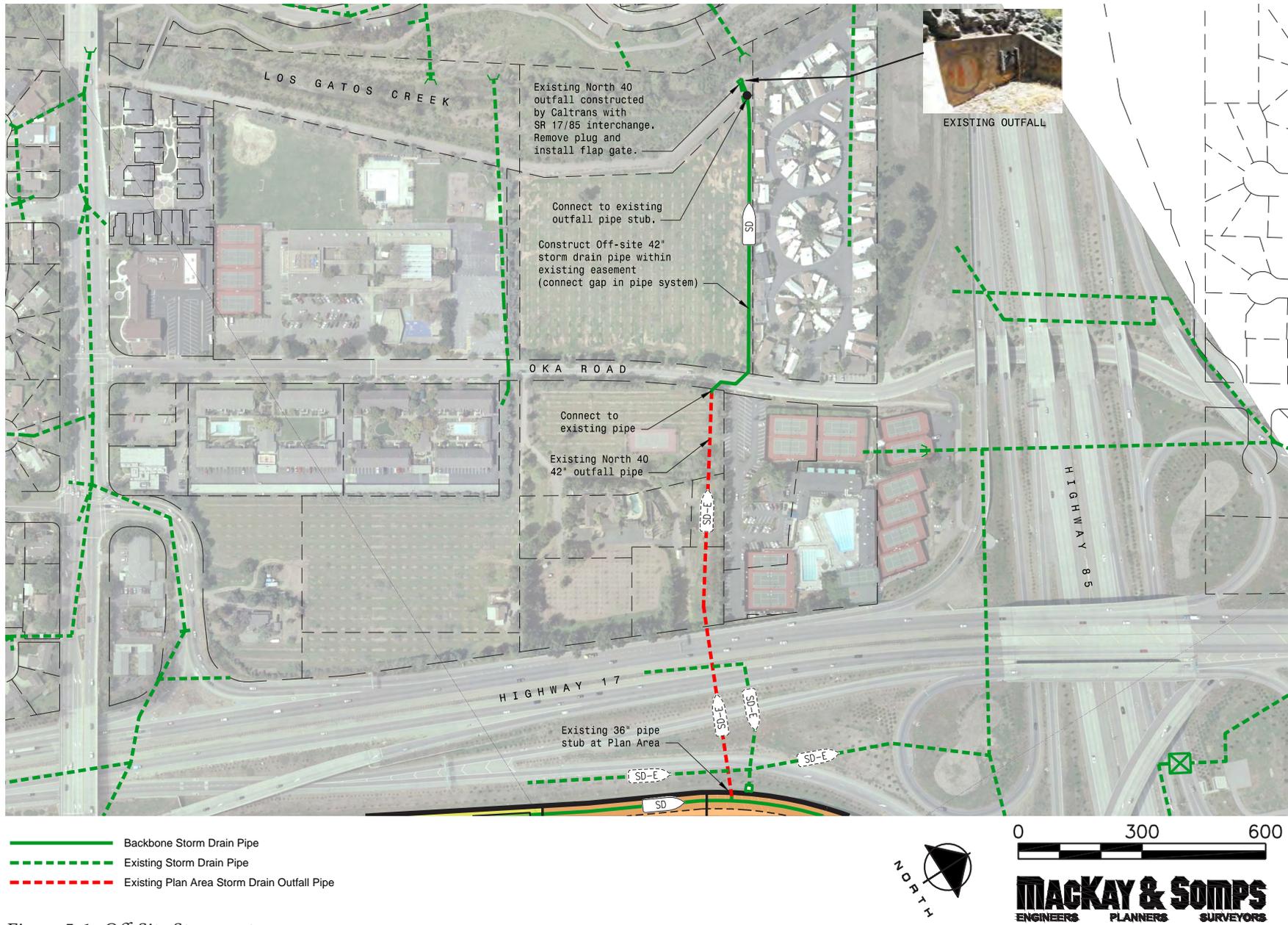


Figure 5-1 Off-Site Stormwater

Los Gatos Boulevard discharge into the drainage system in Los Gatos Boulevard, which ultimately connects to the Caltrans Highway 17/85 interchange system (the existing system in Los Gatos Boulevard is undersized and does not meet current Town standards). The existing developed parcel in the southeast corner of the Specific Plan Area discharges into the drainage system in Lark Avenue. Runoff from the existing undeveloped portion of the Specific Plan Area discharges directly into the Caltrans Highway 17/85 interchange drainage system.

The Specific Plan Area drainage shed consists of a combination of on-site and off-site areas. The on-site area includes all parcels except for two within the Specific Plan Area. The existing 0.3 acre parcel at the southeast corner of the Specific Plan Area that currently drains to the Lark Avenue system will continue to do so after development of the Specific Plan Area. The existing 0.7 acre parcel at the northeast corner of the Specific Plan Area currently drains to the Los Gatos Boulevard system and will continue to do so after development of the Specific Plan Area. The other existing developed parcels will have the ability to connect to the proposed Specific Plan Area drainage system, which will allow them to eliminate pumping up to Los Gatos Boulevard. Therefore the resultant on-site drainage shed area is approximately 42.0 acres. The off-site area that currently drains to the Los Gatos Boulevard system that is undersized is approximately 9.8 acres. The total Specific Plan Area drainage shed including on-site and off-site areas is approximately 51.8 acres.

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) panel number 06085C0239H (effective date May 18, 2009) the Specific Plan Area is classified as Zone 'X'. Zone 'X' for the Specific Plan Area is not subject to inundation from the 1% annual chance flood (i.e. the Specific Plan Area is not within the 100-year flood plain), but is subject to inundation from the 0.2% annual chance flood (i.e. 500-year flood).

5.3.2 PROPOSED STORMWATER COLLECTION AND CONVEYANCE

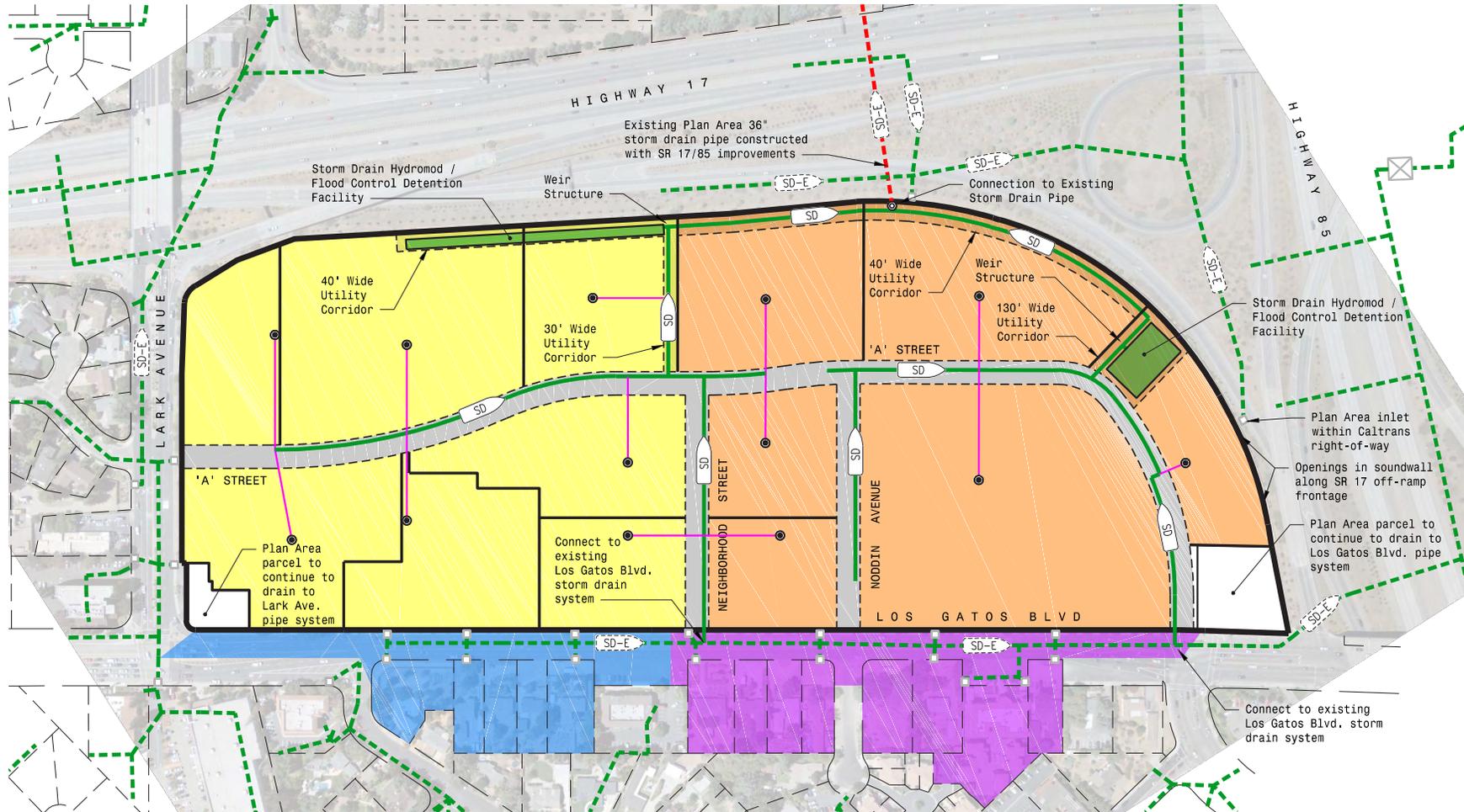
In conjunction with the preparation of this Specific Plan both the Town of Los Gatos Public Works Department (Town) and the Santa Clara Valley Water District (SCVWD) were consulted to determine existing system operation, capacity and future infrastructure needs. The Town and SCVWD reported the following:

- The Town reported that the development of the Specific Plan Area is a regulated project under the Municipal Regional Permit (MRP) Provision C.3 including Provisions C.3.c (low impact development) and C.3.g (hydromodification).
- The Town reported that the drainage system in Los Gatos Boulevard is undersized and therefore drainage from the Los Gatos Boulevard frontage will need to be rerouted through the Specific Plan Area. The Specific Plan Area drainage system will need to be oversized to mitigate existing deficiencies not caused by the development of the Specific Plan Area.
- SCVWD reported that runoff from the Specific Plan Area should not increase the water surface elevation in Los Gatos Creek nor increase the lateral extent of flooding.

Components of the proposed drainage system improvements include water quality, hydromodification, 10-year storm collection, 100-year flood protection and overland release. The County of Santa Clara Drainage Manual has been utilized as the basis of design for the Specific Plan Area for elements that are not dictated by Town standards.

Proposed improvements are located on-site within the Specific Plan Area and at off-site locations. Based on the "Infrastructure Plan Objectives", the Specific Plan Area is divided into two primary

Note: Stormwater system as shown in conceptual only and subject to change



- Shed 'A' On-site Drainage Area
- Shed 'B' On-site Drainage Area
- Shed 'A' Off-site Drainage Area
- Shed 'B' Off-site Drainage area
- Utility Easement (future additional easements will be determined during development application process)
- Underground Hydromodification / Detention Storage Facility
- Backbone Storm Drain Pipe
- Existing Storm Drain Pipe
- Existing Plan Area Storm Drain Outfall Pipe
- Stub Pipe to Conceptual Drainage Management Area (DMA)

NORTH

MACKEY & SOMPS
ENGINEERS PLANNERS SURVEYORS

Figure 5-2 On-Site Stormwater

drainage sheds. Shed 'A' generally covers the south side of the Specific Plan Area and Shed 'B' generally covers the north side of the Specific Plan Area as illustrated in Figure 5-2.

a. Water Quality Improvements:

Water quality concepts for the Specific Plan Area are based on the requirements of Provision C.3.c of the MRP (Low Impact Development). The goal of LID is to reduce runoff and mimic a site's predevelopment hydrology by minimizing disturbed areas and impervious cover and then infiltrating, storing, detaining, evapotranspiring, and/or biotreating stormwater runoff close to its source. Provision C.3.c includes both "source control" and "site design and treatment" requirements.

- Source control measures: consist of either structural project features or operational "good housekeeping" practices that prevent pollutant discharge and runoff at the source (i.e. covered trash enclosures, storm drain inlet stenciling, etc.).
- Site design measures: are techniques that help reduce the size of treatment measures by reducing the effective impervious surface area (i.e. self treating/retaining areas, tree preservation/planting, green roofs, pervious/grid pavements, etc.).
- Stormwater treatment measures: are engineered systems that are designed to remove pollutants from stormwater using processes such as filtration, infiltration and sedimentation (i.e. bioretention areas, flow through planters, tree well filters, subsurface infiltration systems, rainwater harvesting, etc.).

As specified in the SCVURPPP C.3 Stormwater Handbook (April 2012) the Specific Plan Area is anticipated to be divided into "Drainage Management Areas" (DMA). A DMA is a designated area of the site that drains to a stormwater treatment measure with

its boundaries based on grade breaks or barriers. Potential DMA's are illustrated on Figure 5-2. As recommended in the handbook, for planning purposes, it can be assumed that the surface area of each DMA is approximately 4% of the tributary impervious area. Backbone roadways may be designed to function as their own DMA's.

Runoff could be treated with a "Green Street" approach prior to entering stormwater inlets. Examples of green street improvements include landscaped based bioretention areas (i.e. rain gardens) and possibly pervious pavement in select low traffic volume locations. Bioretention areas can be located at curb return bulbouts, mid-block bulbouts and/or within landscaped planting strips between curbs and sidewalks where separated sidewalks are planned. Bioretention designs typically include curb cuts and/or flush curbs at select locations. The MRP and SCVURPPP handbook can be referenced for more information such as sizing criteria, design details, plant selection, mosquito control, etc.

b. Hydromodification:

Hydromodification (HM) concepts for the Specific Plan Area are based on the requirements of MRP Provision C.3.g. Provision C.3.g specifically states:

Stormwater discharges from HM Projects shall not cause an increase in the erosion potential of the receiving stream over the pre-project (existing) condition. Increases in runoff flow and volume shall be managed so that post project runoff shall not exceed estimated pre-project rates and durations, where such increased flow and/or volume is likely to cause increased potential for erosion of creek beds and banks, silt pollutant generation, or other adverse impacts on beneficial uses due to increased erosive force.

The MRP requires HM controls to be designed such that post project stormwater discharge rates and durations match pre-project discharge rates and durations from 10% of the pre-project 2-year peak flow up to the pre-project 10-year peak flow.

It is likely that underground vaults will be constructed within the Specific Plan Area to meet HM requirements. One vault is anticipated per Specific Plan Area drainage shed (i.e. one for the north area and one for the south area). If the vaults are constructed, a flow control device structure will be integrated into the vaults to provide flow control.

c. 10-year event storm collection:

Underground pipe systems within the Specific Plan Area shall be designed to contain the 10-year storm. A conceptual layout of the proposed drainage system is illustrated in Figure 5-2.

100-year event flood control protection:

Flows in excess of the 10-year event may be conveyed in the streets, provided that development is not subject to flooding. Excess stormwater may also be detained in open space areas and parking lots, provided that development is not subject to flooding.

Given that the Specific Plan proposes relatively narrow streets it is possible that portions of the street system may not have the ability to safely convey the 100-year storm event. At these select locations, the drainage system may be designed to contain the 100-year event underground, or other equivalent means will be provided. Inlets at these locations should be designed to allow the tributary 100-year flow rate to enter the storm drain system.

d. Overland release/grading:

A safe overland release must be provided for the design 100-year flow. The County standards indicate that within urbanized areas,

the 100-year discharge may be carried by a combination of a storm drain system and surface flow on the street, as long as the hydraulic grade line is contained within the street rights-of-way.

As indicated in the 100-year event flood control protection section above, the underground storm drain system could be designed to contain the 100-year event. However, a safe overland release shall still be provided for the Specific Plan Area in the event the underground system becomes plugged or gets blocked. Based on the constraints of the existing topographic conditions, the overland release for the Specific Plan Area is through the Caltrans soundwall at the north end of the site, with the Specific Plan Area grading design providing a continuous overland release path from south to north. However, designing the underground storm drain system to collect the 100-year event would minimize the likelihood that overland flow through the Specific Plan Area and soundwall will occur.

5.4 POTABLE WATER

5.4.1 BACKGROUND AND EXISTING CONDITIONS

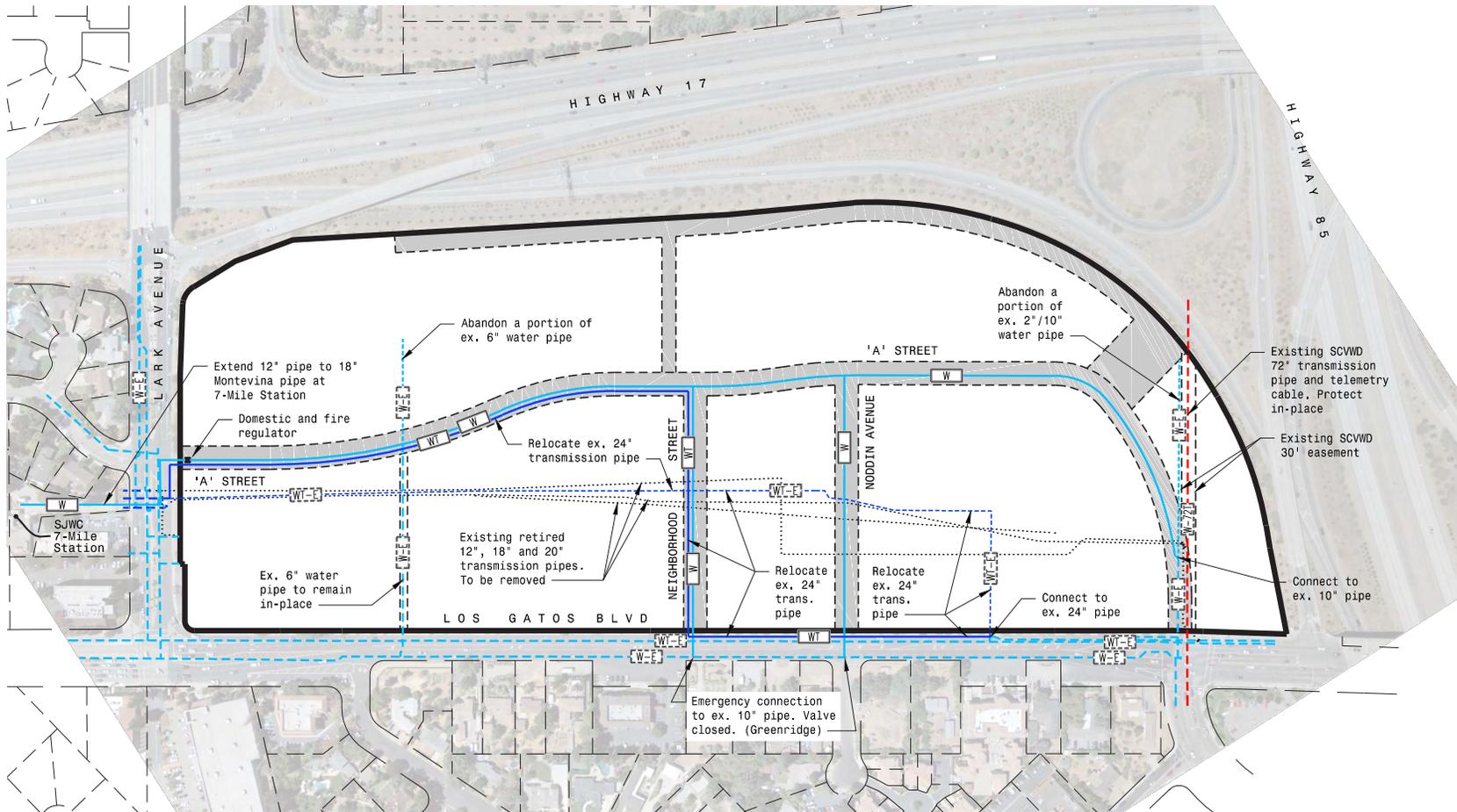
Public water service for the Specific Plan Area is currently and will continue to be provided by San Jose Water Company (SJWC). The SJWC service area encompasses approximately 139 square miles including the Town of Los Gatos, most of the City of San Jose, City of Cupertino, the entire Cities of Campbell, Monte Sereno, Saratoga and parts of unincorporated Santa Clara County. SJWC provides water supply from three sources including groundwater, imported treated surface water and local raw mountain surface water.

SJWC owns and operates water “stations” within the Town of Los Gatos. These stations include tanks, pump stations and regulators that regulate the water pressure. The “7-Mile” station is located just south

5

INFRASTRUCTURE AND PUBLIC FACILITIES

Note: Water system as shown in conceptual only and subject to change



- Utility Easement
- Backbone Potable Water Pipe (SJWC - Distribution)
- Backbone Potable Water Pipe (SJWC - Transmission)
- Existing Potable Water Pipe (SJWC)
- Existing Potable Water Pipe (SCVWD)
- Existing Potable Water Pipe to be Abandoned (SJWC - Transmission)
- Existing Potable Water Pipe to be Abandoned (SJWC - Distribution)
- Existing Retired Potable Water Pipe to be Removed (SJWC)



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Figure 5-3 Potable Water On-Site and Off-Site

of the Specific Plan Area to the south of Lark Avenue and west of Los Gatos Boulevard.

Multiple pressure zones exist within the vicinity of the Specific Plan Area including Montevina, Greenridge, More and Dow. According to the SJWC system map an existing active 24 inch transmission pipe traverses through the undeveloped portion of the Specific Plan Area as illustrated in Figure 5-3. The system map also shows 12 inch and 18 inch transmission pipes through the Specific Plan Area that have been previously retired, but left in-place. According to information provided by an existing property owner, a 20 inch transmission pipe was previously retired within the Specific Plan Area that is not reflected on the SJWC system map. Additional existing transmission pipes are located in Los Gatos Boulevard and Lark Avenue. Existing distribution pipes ranging in size from two inches to 10 inches currently provide service to the Specific Plan Area. The distribution pipes are located within Los Gatos Boulevard, Lark Avenue, Burton Road and Bennett Way as illustrated in Figure 5-3.

Santa Clara Valley Water District owns and operates a 72 inch raw water pipeline (Almaden Valley Pipeline) within the Specific Plan Area. The pipeline traverses the Specific Plan Area within Burton Road. A SCVWD telemetry cable parallels the pipeline.

5.4.2 WATER SUPPLY AND DEMAND

SJWC will provide water to the Specific Plan Area for domestic use, irrigation use, and fire protection. As specified in the Town General Plan average water use is estimated at 250 gallons per day (gpd) per multi-family residential unit, 400 gpd per single-family residential unit and 0.0751 gpd per square feet for commercial or office. The total average projected water demand for the Specific Plan Area is approximately 242 acre-feet per year at build-out.

At the time of this Specific Plan, there are no known existing groundwater wells within the Specific Plan Area; however, if any exist, they will be abandoned as development occurs in accordance with Santa Clara County Department of Environmental Health Standards.

5.4.3 WATER DISTRIBUTION

Based on information provided to SJWC by the Santa Clara Fire Department, the Specific Plan Area water system should be designed to achieve a fire flow of 4,000 gallons per minute (gpm). SJWC prepared a preliminary hydraulic model for the Specific Plan Area, which indicates that a fire flow of approximately 4,200 gpm is available from the Montevina pressure zone. Approximately 420 feet of 12 inch pipe will be extended from the Specific Plan Area at Lark Avenue to an existing 18 inch pipe located at the 7-Mile Station as illustrated on Figure 5-3. Domestic and fire pressure regulators will be required within the Specific Plan Area to maintain 70 pounds per square inch (psi) for domestic use and 60 psi for fire protection.

The backbone water system is intended to extend from Lark Avenue through the Specific Plan Area and out to Los Gatos Boulevard. The system will include an emergency connection to an existing pipe in Los Gatos Boulevard (Greenridge Zone). A combination of 12 inch, 10 inch and eight inch pipes will be located within Specific Plan Area streets and will be designed to provide an internal looped system to increase system reliability.

5.4.4 WATER TRANSMISSION PIPES

The previously retired SJWC transmission pipes within the Specific Plan Area will be removed to avoid conflicts with proposed development. The active 24 inch transmission pipe will be relocated as illustrated on Figure 5-3. The new alignment will be within proposed streets within the Specific Plan Area and within Los Gatos Boulevard.

The existing SJWC easements are anticipated to be quit-claimed in exchange for a new easement following the proposed pipe alignment. The existing SCVWD 72 inch transmission pipe and associated telemetry cable are planned to be protected in place.

5.5 WASTEWATER

5.5.1 BACKGROUND AND EXISTING CONDITIONS

Public wastewater collection service for the Specific Plan Area is currently and will continue to be provided by West Valley Sanitation District (WVSD). The WVSD service area encompasses the Town of Los Gatos and the Cities of Campbell, Monte Sereno, much of Saratoga and some unincorporated areas of the county within the district boundary.

The WVSD's wastewater collection system consists of main and trunk sewers as well as sewer laterals, which are maintained and operated by the WVSD. The WVSD's system within the Town of Los Gatos consists primarily of gravity mains with the collection system flowing north, exiting the Town limits through multiple trunk sewers. These systems continue through the City of San Jose trunk sewers and ultimately to the San Jose/Santa Clara Water Pollution Control Plant for wastewater treatment and disposal.

Wastewater treatment for the Specific Plan Area will occur at the San Jose/Santa Clara Water Pollution Control Plant. The Plant is one of the largest advanced wastewater treatment facilities in California. It treats and cleans the wastewater of over 1,500,000 people that live and work in the 300-square mile area encompassing San Jose, Santa Clara, Milpitas, Campbell, Cupertino, Los Gatos, Saratoga, and Monte Sereno. The Plant has the capacity to treat 167,000,000 gallons of wastewater per day. It is located in Alviso, at the southernmost tip of the San Francisco Bay.

Gravity pipelines owned and operated by WVSD exist within portions of Los Gatos Boulevard and Lark Avenue as illustrated on Figure 5-4. Developed properties within the Specific Plan Area are connected to the existing pipes. The undeveloped portion of the Specific Plan Area utilizes septic systems for disposal. Developed properties along Los Gatos Boulevard will continue to discharge wastewater to the existing system within the road.

An existing sewer pipeline was constructed under Highway 17 by Caltrans as part of the Highway 17/85 interchange project. The pipeline extends from the Specific Plan Area sloping to the west and ends as a stub pipe near Oka Road as illustrated on Figure 5-5. The pipeline connection to Oka Road was designed and approved by the Town and WVSD, but the connection was not completed.

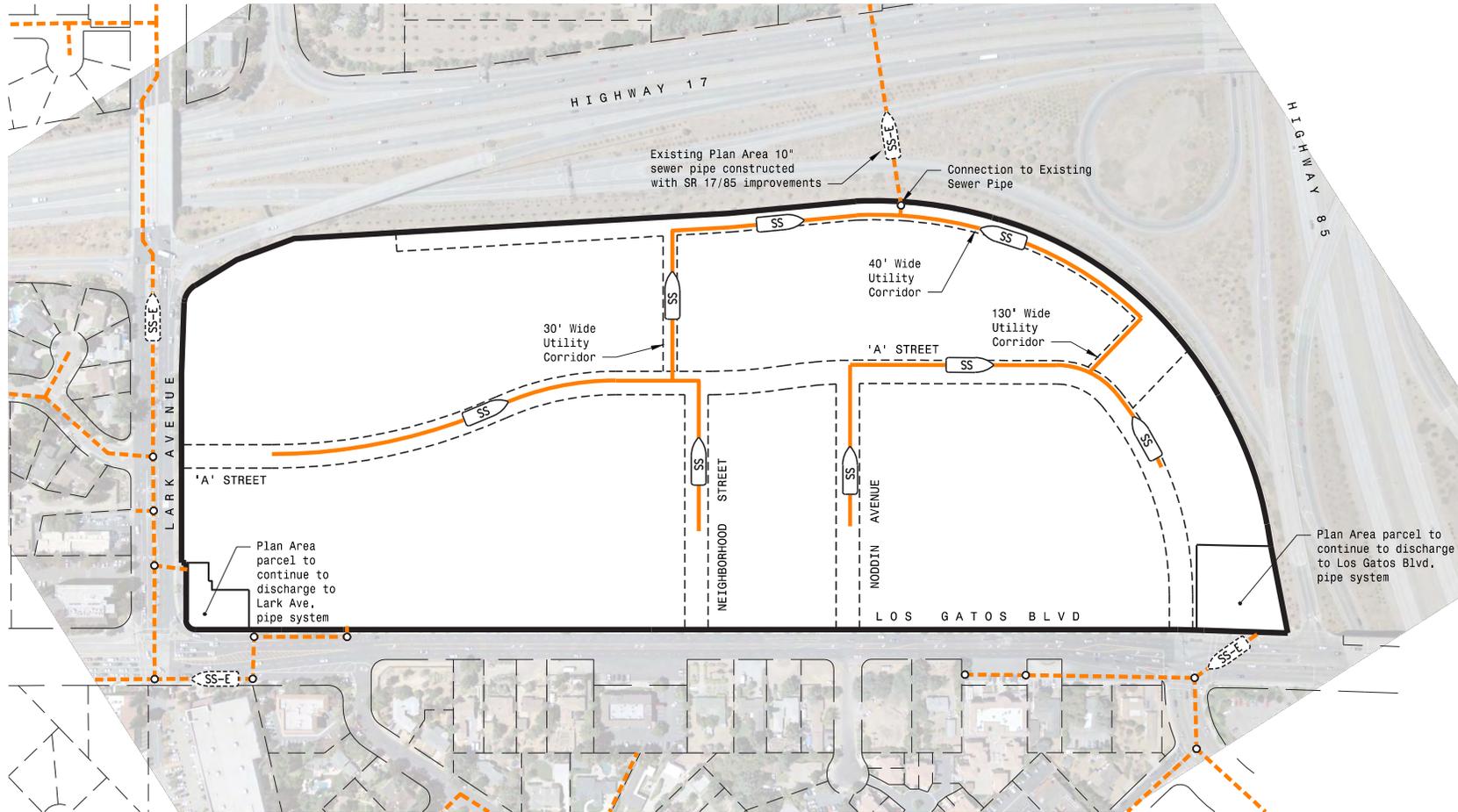
5.5.2 WASTEWATER GENERATION AND TREATMENT

Based on information provided by WVSD residential units are assumed to generate 250 gallons per day (gpd) per unit. Commercial/retail is assumed to generate 70 gpd per 1,000 square feet. The total average projected wastewater demand for the Specific Plan Area build-out is approximately 131,600 gallons per day.

The WVSD and San Jose/Santa Clara Water Pollution Control Plant have adequate treatment capacity to accommodate the development of the Specific Plan Area.

Existing private septic systems and/or leech fields within the Specific Plan Area will be abandoned as development occurs in accordance with Santa Clara County Department of Environmental Health standards.

Note: Wastewater system as shown in conceptual only and subject to change



-  Utility Easement
-  Backbone Sewer Pipe
-  Existing Sewer Pipe

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Figure 5-4 On-Site Wastewater



- Backbone Sewer Pipe
- - - Existing Sewer Pipe



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Figure 5-5 Off-Site Wastewater

5.5.3 WASTEWATER COLLECTION

Gravity pipelines will be constructed in roadways within the Specific Plan Area. The Specific Plan Area pipes will connect to the existing stub adjacent to Highway 17. The pipe under Highway 17 will be connected to the existing sewer pipe in Oka Road as illustrated on Figure 5-5. A Sewer Impact Evaluation study analyzing downstream pipe capacity was prepared in March 2014. This study shows that there is adequate capacity to accommodate the proposed Specific Plan development within the existing 21-inch pipe.

5.6 DRY UTILITIES

5.6.1 BACKGROUND AND EXISTING CONDITIONS

Primary electric power and natural gas service to the Specific Plan Area is currently provided by Pacific Gas & Electric (PG&E). Power is provided by a combination of underground and overhead facilities. Telephone service is provided by Verizon and cable television service is provided by Comcast. A significant amount of utilities exist under the Los Gatos Boulevard sidewalk along the Specific Plan Area frontage including: electric, gas, phone, cable, Verizon fiber optic line, Town fiber optic line for traffic signal interconnects and street light conduits. Multiple utilities exist along Lark Avenue, Bennett Way, and Burton Road as well as utility extensions to buildings that are not located within streets.

5.6.2 ELECTRICITY AND NATURAL GAS

PG&E will provide electric and natural gas service to the Specific Plan Area. An underground common “joint trench” will be constructed within streets within the Specific Plan Area. Existing overhead power

lines will be undergrounded or removed in accordance with Town requirements. Gas pipes no longer needed within the Specific Plan Area will be removed.

5.6.3 TELECOMMUNICATION AND CABLE

Verizon will provide telephone service to the Specific Plan Area and Comcast will provide cable television service. Telephone and cable lines will be located in the common “joint trench” with electric and gas.

5.7 SOLID WASTE

All waste generated in the Town of Los Gatos is collected by West Valley Collection and Recycling, LLC and serviced by three local landfills. There is sufficient capacity in the existing landfill system to handle the anticipated waste generated by the Specific Plan Area. Potential developments under the proposed Specific Plan are required to adhere to all local, regional and state standards and policies regarding waste reduction and recycling.

5.8 PUBLIC FACILITIES

5.8.1 PARKS AND OPEN SPACE

Several parks are located within proximity to the Specific Plan Area and are serviced by the Town of Los Gatos Parks and Public Works Department. The Specific Plan incorporates a 30% open space requirement and additional goals and policies, (described in Chapter 2) to service the Specific Plan Area. Required common open space may be used for public benefit, but will be privately owned and maintained. Residential development will provide pocket parks and additional common amenities while commercial properties are encouraged to include plazas and other outdoor gathering spaces.

5.8.2 PUBLIC SCHOOLS

There are no schools located within the boundaries of the Specific Plan, however the Specific Plan Area will be serviced by the Los Gatos Union School District, Campbell Union School District, Los Gatos – Saratoga Union High School District, and Campbell Union High School District.

The following is a summary of two studies that were conducted to determine student generation for the Specific Plan Area.

a. Student Projections

The following summarizes two separate processes that the Town, the Los Gatos School Districts, and Campbell School Districts used to derive student population projections for the Town, including the Specific Plan Area.

Town 2020 General Plan EIR Student Generation Rates: The first set of data is from the Town’s adopted 2020 General Plan EIR. This data was developed in 2010, using both Los Gatos Union School District (LGUSD) and Los Gatos-Saratoga Joint Union High School District (LGSJUHSD) actual enrollment and some data from another high performing school district. The General Plan EIR data is based on housing types, including single-family detached, attached (condominiums and townhomes), apartments, below market rate apartments, or integrated mixed-use. Since the Town did not have any large developments that were exclusively below market rate apartments or integrated mixed-use to survey, this data was derived from the Irvine School District, which has a similar Academic Performance Index.

Based on the previous proposal of a maximum 364 residential units, if the units were a combination of attached product types (Townhouse and Condominium), the development would generate 67 children in grades K through 12 at any given time

once all residential product types were built and occupied, which would be in a timeframe of 2015 to 2020. This same number of single-family units (364) would generate 205 children in grades K through 12. However, the maximum allowed number of residential units is limited to 270 units which would yield a lower number of new students.

LGUSD draft Student Population Projections for 2012/13 to 2021/22: In July 2012, the LGUSD released a recently prepared Student Population Projections for 2012/13 to 2021/22. This report assumes four different projection assumptions, including Projection A (no future development), Projection B (only development that has been approved), C (all development in B + development in early planning stages, including the Specific Plan Area, but a 50% of maximum build-out), and D (all of Projection C, but at 100% build-out). The study further used existing development to define the generation rates. This included Bella Vista on Bersano Lane to represent single-family, Creekside Village off of Woodland Avenue to represent an attached product and Riviera Apartments on Riviera Terrace for an apartment housing type. The report categorizes the Specific Plan Area in the Creekside Village product category, and based on this classification indicates that the Specific Plan Area would generate 53 K-12 children by 2022 for Projection C and 106 K-12 children by 2022 for Projection D. It is important to note that these LUGSD projections are based on 400 residential units, and the Town Council reduced this number to a maximum of 270 units at the April 14, 2015 meeting.

Residential and commercial developments within the Specific Plan Area would be required to pay school impact fees to reduce impacts to the schools system.

b. School Funding

While the number of students anticipated is minimal, the School District is expected to benefit from higher property tax revenues with the development of the site. The majority of the Specific Plan Area property is currently zoned agriculture and is assessed at a lower rate than developed property. The sale and development of the property will increase the annual property tax revenue that is paid to the schools. Additionally, the development of the property would result in payment of one time impact fees as set by each School District.

5.8.3 LOS GATOS/MONTE SERENO POLICE DEPARTMENT

The Los Gatos/Monte Sereno Police Department provides service to the Town of Los Gatos and Monte Sereno. The department is a full service organization comprised of 64 sworn and civilian personnel and over 150 community volunteers. The police department operates from two locations: patrol and detective services are located in an office on Los Gatos Boulevard near Blossom Hill Road, about one mile from the Plan Area, and all other police services are located at the Town Hall, 110 East Main Street, about 2.5 miles from the Plan Area. The primary facility is located at the Town Hall and contains the following bureaus, units, and operational teams:

- Detective Bureau
- Records and Communications Bureau
- Personnel and Community Services Bureau
- Crime Analysis
- School Resource and D.A.R.E.
- Traffic and Motorcycle Unit

- Canine Team
- SWAT and Hostage Negotiation Teams
- Bicycle Patrol Team
- Evidence Team

5.8.4 SANTA CLARA COUNTY FIRE DEPARTMENT

The Santa Clara County Fire Department provides ISO Class 2/5 services for Santa Clara County, California and the communities of Campbell, Cupertino, Los Altos, Los Altos Hills, Los Gatos, Monte Sereno, and Saratoga. The department also provides protection for the unincorporated areas adjacent to those cities. The department includes 17 fire stations, an administrative headquarters, a maintenance facility, five other support facilities, 21 pieces of apparatus and four command vehicles, to cover 114.5 square miles (297 square km) and a population of over 252,000. The department employs over 307 fire prevention, suppression, investigation, administration, and maintenance personnel; daily emergency response consists of 71 employees. The department's suppression force is also augmented by approximately 40 volunteer firefighters. The Town of Los Gatos is home to the Administrative Headquarters and the Winchester Fire Station, Shannon Fire Station, and Los Gatos Fire Station.

The nearest fire stations are located at Winchester Boulevard and Lark Avenue, about 0.6 miles from the Plan Area, and on Shannon Road east of Los Gatos Boulevard, about 1.5 miles south of the Plan Area (Santa Clara County Fire Department 2010).

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6. PLAN IMPLEMENTATION, PHASING, AND ADMINISTRATION

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6. PLAN IMPLEMENTATION, PHASING, AND ADMINISTRATION

6.1 INTRODUCTION

This chapter addresses the actions that are necessary to implement the Specific Plan by both the Town and private investment in order to achieve the goals and objectives outlined in this document. The Specific Plan is a tool to guide future development in the Specific Plan Area to facilitate the development of a pedestrian-oriented neighborhood supported by a mix of uses and activities. Outlined in this chapter is a summary of general phasing, potential funding sources, and Specific Plan administration. Each of these actions will be guided by various Town departments and implemented through private investment. The phasing identified is meant to be used as a guide and is not intended as a mandatory requirement to achieve the final vision and objectives for the Specific Plan Area. In certain cases further study and analysis may be required to find realistic and timely solutions to implement the recommendations.

The contents of this chapter include:

- A discussion regarding the anticipated phasing of development within the project area;
- A summary of potential funding sources that may be available for implementation of various Specific Plan recommendations;
- Administrative processes and requirements necessary to implement the Specific Plan; and
- A description of the authority of the Specific Plan and the administrative process required for amendments or modifications to the Specific Plan.

6.2 PHASING

It is anticipated that the Specific Plan will be implemented over time and in more than one phase. Each phase shall stand alone and shall not be dependent on improvements required in future phases. The proposed phasing plan is designed to achieve the following objectives:

- Each development phase will include adequate existing and/or planned public and/or private utilities and services, and related infrastructure to support the demand for such utilities and services generated by each phase to enable it to stand alone; and
- Critical roadway improvements necessary to provide access into the Specific Plan Area will be constructed in each phase of development.
- All infrastructure necessary to support the development shall be constructed in a capacity that allows for current and future phases, subject to approval of the Town Engineer. All infrastructure stubbed for future use, including utilities and roadways, shall be constructed so as not to appear unfinished.
- All construction of private and public infrastructure will be funded by the developer(s) at the time of construction based on the proposed phasing. All on-site infrastructure improvements required by the project will remain private with all ongoing maintenance responsibilities belonging to the developer or a successor entity established for this purpose and approved by the Town Engineer.
- The project will be required to pay development impact fees, such as Traffic Impact Fees and School Impact Fees. The amount of development impact fees to be paid by the developer is set forth by the Town and other agencies adopted fee schedules.

The Town cannot at this time predict when or the rate at which phases will be developed with certainty. A preliminary phasing diagram (Figures 6-1, 6-2, and 6-3) has been prepared based upon current information regarding infrastructure conditions and property ownership patterns.

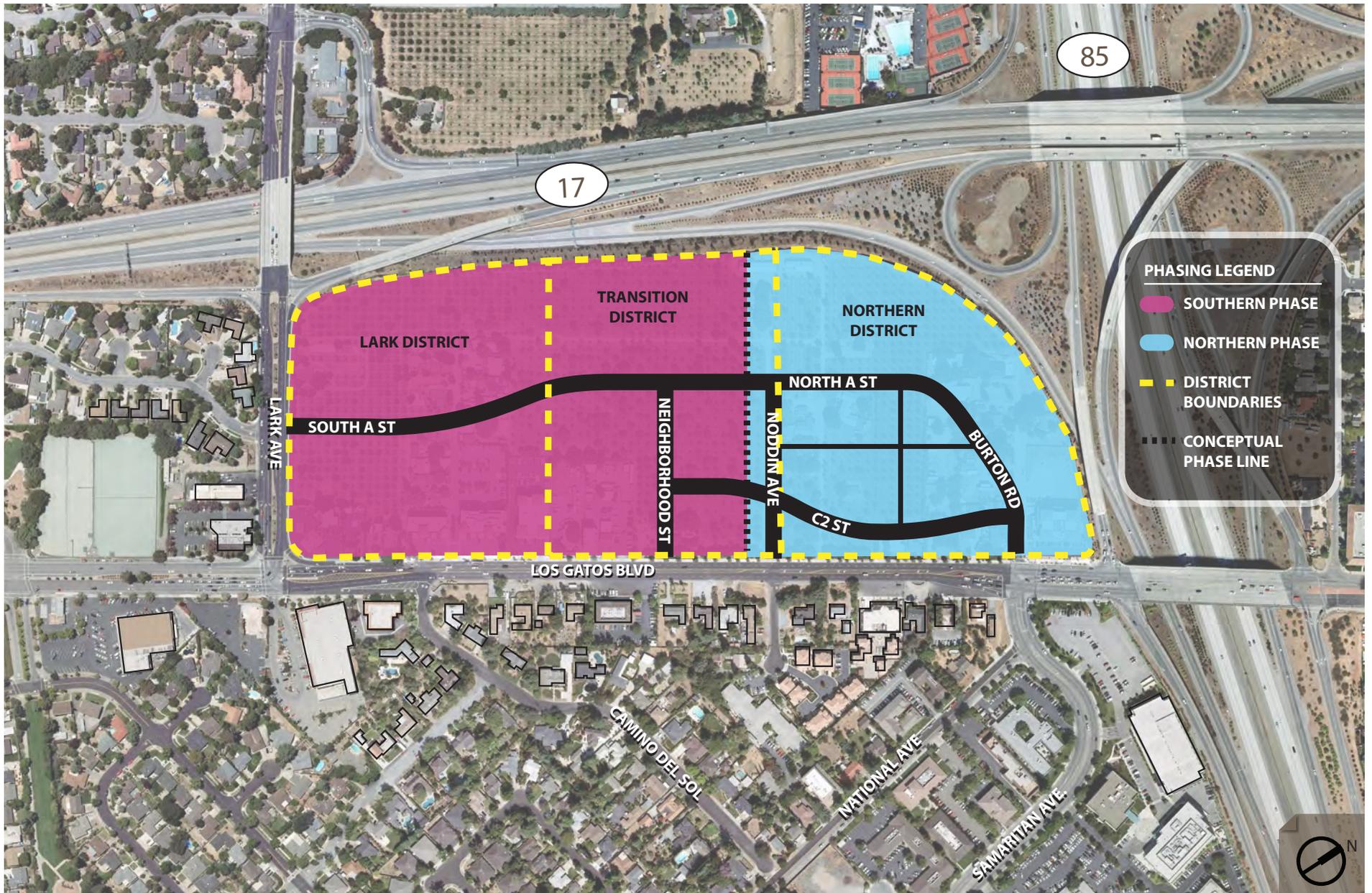
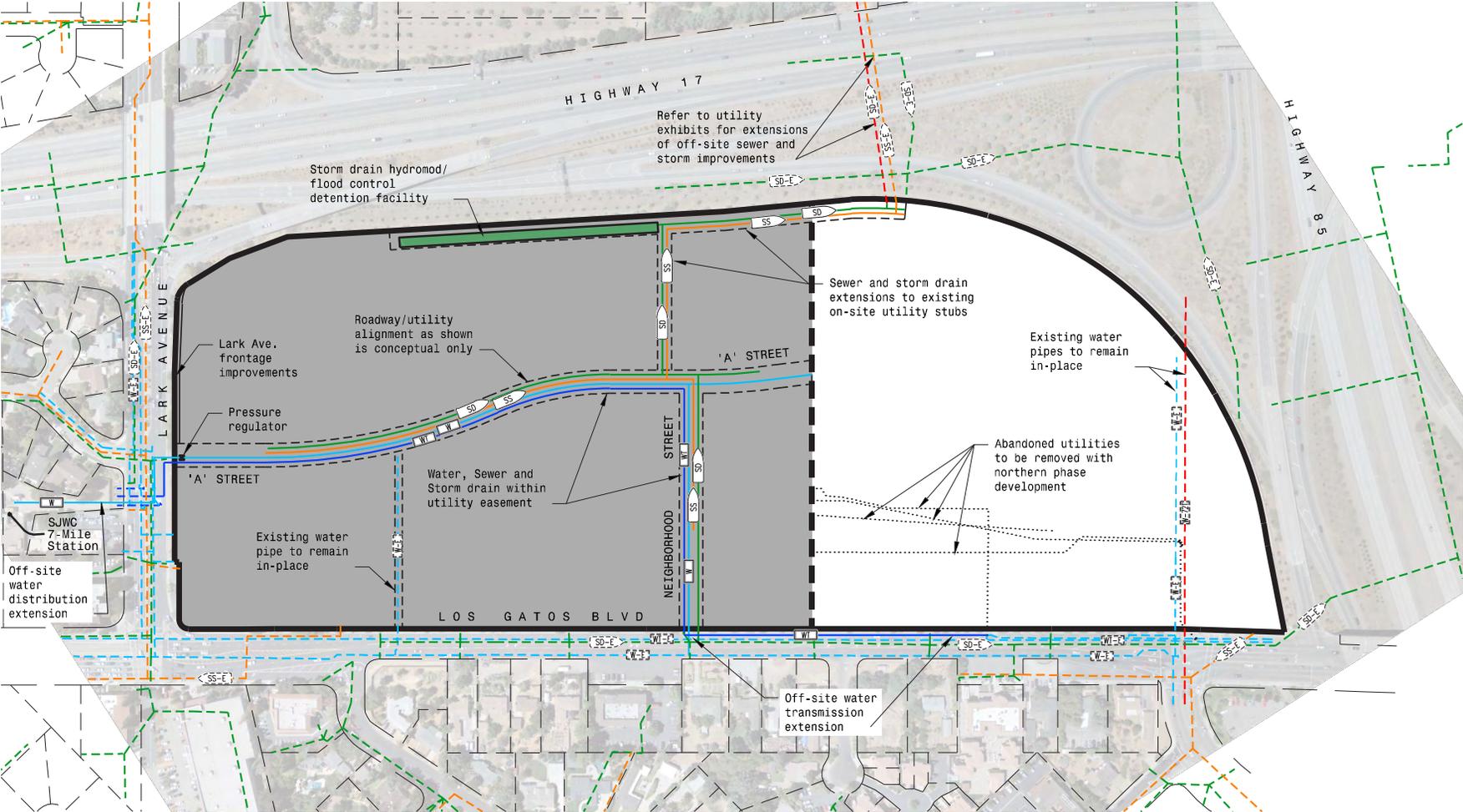


Figure 6-1 Conceptual Phasing Areas



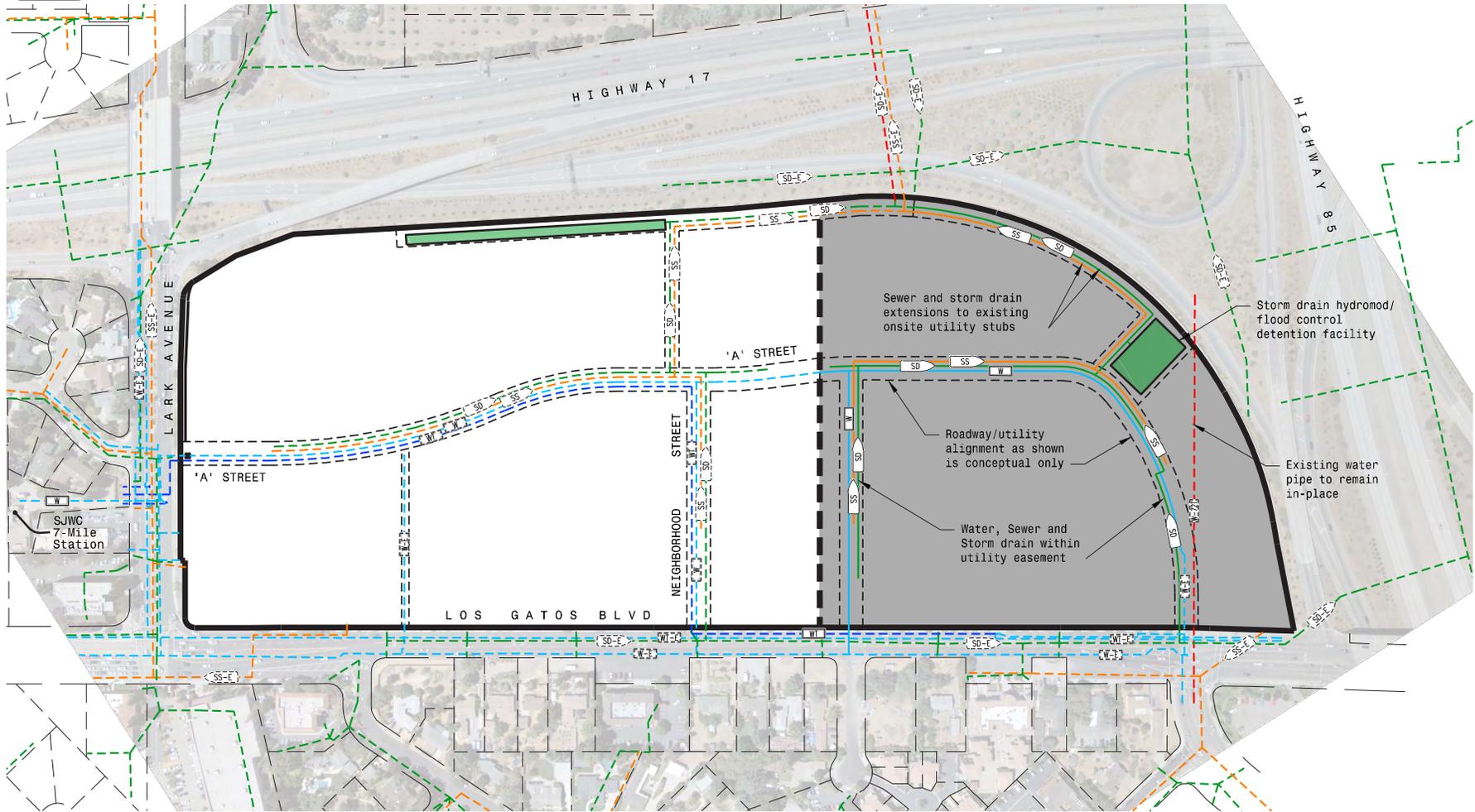
- Southern Phase Development Area
- Conceptual Phase Line
- Backbone Potable Water Pipe (SJWC - Distribution)
- Backbone Potable Water Pipe (SJWC - Transmission)
- Existing Potable Water Pipe (SJWC)
- Existing Potable Water Pipe (SCVWD)
- Existing Potable Water Pipe to be Abandoned (SJWC - Transmission)
- Existing Potable Water Pipe to be Abandoned (SJWC - Distribution)
- Existing Retired Potable Water Pipe to be Removed (SJWC)
- Backbone Storm Drain Pipe
- Existing Storm Drain Pipe
- Existing Plan Area Storm Drain Outfall Pipe
- Stub Pipe to Conceptual Drainage Management Area (DMA)
- Backbone Sewer Pipe
- Existing Sewer Pipe

Note: Additional detail provided in Chapter 5 of this document

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Figure 6-2 Conceptual Phase 1 - Southern Area



- Northern Phase Development Area
- Conceptual Phase Line
- Backbone Potable Water Pipe (SJWC - Distribution)
- Backbone Potable Water Pipe (SJWC - Transmission)
- Existing Potable Water Pipe (SJWC)
- Existing Potable Water Pipe (SCVWD)
- Existing Potable Water Pipe to be Abandoned (SJWC - Transmission)
- Existing Potable Water Pipe to be Abandoned (SJWC - Distribution)
- Existing Retired Potable Water Pipe to be Removed (SJWC)

- Backbone Storm Drain Pipe
- Existing Storm Drain Pipe
- Existing Plan Area Storm Drain Outfall Pipe
- Stub Pipe to Conceptual Drainage Management Area (DMA)
- Backbone Sewer Pipe
- Existing Sewer Pipe

Note: Additional detail provided in Chapter 5 of this document



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Figure 6-3 Conceptual Phase 2 - Northern Area

6.3 FINANCING

All private and public infrastructure, roadways, and open space will be funded by the developer(s) at the time of construction as described in Section 6.2, Phasing, of this Specific Plan. The Environmental Impact Report will analyze if there are adequate Town services to meet the demands of the Specific Plan. If there are not adequate Town services, funding mechanisms will need to be in place to enable police protection, fire protection, maintenance, engineering services, and administration/overhead functions (including Town Council, Town Manager, Town Attorney, Town Clerk, and Finance). At this time the exact extent of the fiscal impacts has yet to be determined; however, it is expected that property and sales tax revenue from the development will cover the Town's costs of providing services. The sale and development of the property will significantly increase the annual property tax revenue and will result in the payment of one-time impact fees. The following sections include a number of potential funding mechanisms that can be considered as a part of plan implementation.

6.3.1 PRIVATE FINANCING

The proposed infrastructure and open space devoted to public use will be privately maintained and will therefore not result in any fiscal impacts to the Town.

6.3.2 GENERAL FUND REVENUE

Fees collected in the Town's General Fund include sales taxes, transient occupancy taxes, property taxes, motor vehicle license fees and other revenue sources. These funds can be utilized for Town services (police, fire, etc.) and capital improvements.

6.3.3 DEVELOPMENT FEES AND EXACTIONS

Development fees may include impact fees imposed pursuant to the Mitigation Fee Act for public facilities; school impact fees; park dedication fees; capacity charges for water and sewer services; charges imposed individually on a project based on an analysis of the project's impacts; and others. While the statutory requirements vary for each of these fees, as a general rule, where the fees are imposed Town-wide on a class of projects, they must be based on findings of a reasonable relationship between the impacts of the development paying the fee, the amount of the fee, and the use of fee revenues. If the fees are imposed on an individual project, the fees must be roughly proportional to the impact of that specific project. Development fees are one-time charges that are paid incrementally as development occurs but cannot be used to fund existing deficiencies in the area or maintenance.

'Exactions' are requirements for dedication of land or construction of improvements, such as road widening or community facilities, that are also based on the impacts of the project. They may be imposed generally, as through parkland dedication ordinances and subdivision improvement requirements, or on an individual project based on the impacts of that project. Credits can be given for land dedication and for capital facilities that would otherwise be supported with impact fees. For instance, a development project could dedicate land or make certain improvements and receive a credit against the impact fee due. Alternatively, if the cost of the facility were to exceed the developer's obligation, the developer would be entitled to enter into a reimbursement agreement with the Town for the amount that exceeds the developer's fee obligation, which would be repaid by future developers.

6.3.4 PROPERTY OWNER/DEVELOPER CONTRIBUTIONS

Property owners or developers may request that Town enter into a development agreement at the Town's sole discretion. Developer(s) may request that the Town agree to 'vest' the existing development approvals for a period of years in exchange for additional public benefits provided by the developer, subject to negotiations.

Development agreements are contracts that must be voluntarily entered into by the Town and the property owner or developer.

6.3.5 COMMUNITY FACILITIES DISTRICT

The Mello-Roos Community Facilities Act of 1982 authorizes cities and towns to establish Community Facilities Districts (CFD) which aid in the financing of public services and facilities through a special tax. This is one form of potential financing for public infrastructure. Most commonly they are established by landowner vote on largely vacant land with fewer than 12 registered voters and used to help finance the infrastructure and services required for new development. A CFD on the North 40 could finance virtually any public facility and additionally could fund new police, fire, park and street maintenance, flood protection, and storm drainage maintenance costs required to serve the project. The special tax would be paid by landowners in the CFD. Because a CFD imposes a tax rather than an assessment (discussed below), the per parcel taxes do not need to be based on the specific benefits to each property owner, making the taxes easier to impose and administer and less likely to be challenged than if an assessment district were established to finance the same facilities or service costs.

6.3.6 BENEFITS ASSESSMENT DISTRICT

Another form of financing for public infrastructure is an Assessment District. Assessment districts are usually formed to pay for infrastructure projects where there is a clear special benefit to individual properties within the district. The owners contribute based only on the special benefits that they receive directly and cannot be charged for benefits accruing to other properties.

The costs for maintaining the project's on-site infrastructure could be funded by an assessment district rather than through a CFD and could cover the following costs:

- Street light electricity and maintenance
- Storm drain maintenance
- Common open space areas and landscaping (e.g. street medians, etc.)
- Street maintenance

The developer could pay for such improvements and the debt would then be paid by assessing those property owners who would be served by the improvements. However, an individual property owner's portion of the debt is based on the owner's special benefit. In recent years the courts have applied strict requirements to determining special benefit, and assessments are more likely to be challenged than special taxes imposed through a CFD.

6.4 ADMINISTRATION

This section describes the authority of the Specific Plan, the process which will be used to consider development applications, and the administrative procedures required for amendments and/or modifications to the Specific Plan.

6.4.1 SPECIFIC PLAN ADMINISTRATION

Proposed developments within the Specific Plan Area will be reviewed pursuant to the established Architecture and Site Review and approval process as defined within Division 3 of the Zoning Ordinance. In addition, proposed developments will be required to adhere to existing Zoning Ordinance regulations and processes for other types of discretionary review, such as those for conditional use permits, variances, and subdivisions.

Future Applications must be consistent with this specific plan standards or their intent and within the parameters studied in the EIR.

6.4.2 RELATIONSHIP TO ZONING REGULATIONS

The land use and development requirements contained in this Specific Plan constitute the primary zoning and development regulations for the Specific Plan Area. These regulations are applied in addition to the Zoning Regulations provisions of the Zoning Ordinance (Chapter 29). If there is a difference or conflict between the requirements of the Town's Zoning Regulations and this Specific Plan, the provisions of this Specific Plan shall prevail.

6.4.3 GENERAL PLAN AMENDMENTS

To be consistent with the Town's Vision Statement and Guiding Principles Developed in 2012, two General Plan guidelines and one policy that addresses the North 40 planning area will be amended. Refer to Section 1.4 of the Specific Plan for revised language.

6.4.4 INTERPRETATION OF PROVISIONS AND USES

The Director of Community Development shall have the responsibility to interpret the provisions of this Specific Plan. If an issue or situation arises that is not sufficiently provided for or is not clearly understandable, those regulations of the Zoning Ordinance that are most applicable shall be used by the Director as guidelines to resolve the issue or situation. This provision shall not be used to permit uses or procedures not specifically authorized by this Specific Plan or the Zoning Ordinance.

If ambiguity arises concerning the appropriate classification of a particular land use, the Director shall have the authority to make an interpretation as to whether the use should be allowed because it is similar to other allowed uses in a particular Specific Plan District, or whether the use should not be allowed in any Specific Plan District.

The Director may refer any matter for interpretation to the Planning Commission for their consideration. The Planning Commission shall make the final decision on an interpretation. The Planning Commission's decision may be appealed to the Town Council.

6.5 SPECIFIC PLAN AMENDMENTS

Amendments to the Specific Plan may be initiated by an individual North 40 property owner or their designee, or by the Town. Additionally any member of the public can make a request to the Town Council for the Town to consider an amendment to the North 40 Specific Plan. The Director of Community Development or his/her designee is responsible for making the determination of whether an amendment to the Specific Plan text or map is needed. Amendment procedures are described below:

- Proposals to amend the Specific Plan must be accompanied by detailed information to document the change required. This information should include revised Specific Plan text and a revised diagram, where relevant, depicting the amendment requested.
- The Town has conducted a comprehensive analysis and invested a significant amount of time and money in the preparation of the Specific Plan, therefore, any proposals to amend the Specific Plan must document the need for such changes. The Town and/or applicant should indicate the economic, social or technical issues that generate the need to amend the Specific Plan. Costs incurred for the amendments shall be the responsibility of the party requesting the amendment.
- The Town and/or applicant must provide an analysis of the amendment's impacts relative to the adopted California Environmental Quality Act (CEQA) documentation. Depending on the nature of the amendment, supplemental environmental analysis may be necessary. The need for such additional analysis shall be determined by the Town of Los Gatos in accordance with the State CEQA Guidelines (Section 15162).

Amendments require an advisory recommendation by the Planning Commission and approval of the Town Council. Planning Commission and Town Council shall each hold public hearings on a proposed Specific Plan amendment, in accordance with Government Code Section 65453.

Examples of Specific Plan amendments include:

- The introduction of a new land use designation not contemplated in the Specific Plan.
- Changes to the circulation system or other community facility which would materially affect a planning concept detailed in the Specific Plan.
- Changes or additions to the design guidelines that the Director finds would alter the stated intent of the Specific Plan.
- Any change that would result in new significant adverse environmental impacts not previously considered in the CEQA compliance documentation for the Specific Plan.
- Changes in dimensional standards except those permitted through a Conditional Use Permit that the Director finds would alter the stated intent of the Specific Plan.

6.6 GLOSSARY

ADJACENT

Nearby or close to, but not necessarily abutting.

ALLEY

A public or private way reserved primarily for vehicular service access to the rear or side of properties otherwise abutting a street.

ALTER

To change, modify, or add to in construction, or to change in size, shape, character, occupancy, or use, a building or structure.

ARTICULATION

The visible expression of architectural elements which, through their form or materials, break up the mass and scale of buildings.

AUTO COURTS

A paved open space, surrounded on three sides by residential structures, and serving as access to garages for those dwelling units. It may also provide access to residence entries.

BALCONY

An exterior platform that projects from or into the façade of a building, and is surrounded by a railing, balustrade or parapet.

BAY WINDOW

A large window or grouping of windows projecting and cantilevering from the outer facade of a building and forming an alcove in the interior of the building.

BELOW MARKET PRICE

Any residential dwelling unit designated for very low, low, and/or moderate income under rules of Chapter 29 of the Town Zoning Ordinance.

BIORETENTION AREAS

Bioretention areas, or rain gardens, are landscaping features adapted to provide on-site treatment of stormwater runoff. They are commonly located in parking lot islands or within small pockets of residential land uses. Surface runoff is directed into shallow, landscaped depressions. These depressions are designed to incorporate many of the pollutant removal mechanisms that operate in forested ecosystems.

BUILDING

Any structure used or intended to be used to shelter a person or personal property.

BULBOUT

Location where the sidewalk edge is extended from the prevailing curb line into the roadway at sidewalk grade, effectively increasing pedestrian space; also referred to as a curb extension.

BULKHEAD

The portion of the building between the bottom of the storefront window and the adjacent outdoor ground level.

BUSINESS OR COMMERCE

The purchase, sale or other transaction involving the handling or disposition of any article, substance or commodity for either profit or livelihood, regardless of whether there is any other motivation. The activities normally conducted in office buildings, offices, and in shops for the sale of personal services are included in the meaning of business or commerce.

CARPORT

An open-sided, roofed, automobile shelter, usually formed by extension of the roof from the side of a building.

CELLAR

An enclosed area that does not extend more than four feet above the existing or finished grade in any location. Cellars, as defined here, shall not be included in the floor area ratio calculation for residential developments. That area of a cellar where the building height exceeds four feet above existing or finished grade shall not be included in this definition and shall be included in the floor area ratio calculation. For purposes of this definition whichever grade (existing or proposed) results in the lowest building profile of a building shall be used.

COMMON GREEN

A grass or landscaped area centrally located for use of residents of the development, their guests, and/or the public.

COMMON OPEN SPACE

A usable open space for the exclusive use of residents of the development, their guests, or open to the public. This space can be either green space or hardscape.

COMPLETE STREETS

Complete streets are roadways designed and operated to enable safe, attractive, and comfortable access and travel for all users, including pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities.

CONDOMINIUM

Condominium, residential means a residential development, a condominium project, a community apartment project or a stock cooperative as defined in Title 6 Common Interest Developments, Section 1351 of the Civil Code. (Condominiums are a form of ownership rather than a housing type)

CONDITIONAL USE PERMIT (CUP)

An authorization allowing a particular use at a specified location, subject to conditions set forth in the authorization and in Chapter 29 of the Zoning Ordinance.

COTTAGE CLUSTER HOUSING

A collection of small houses arranged around and fronting onto a common green space. Units are usually smaller than typical single family homes. Parking is provided in consolidated parking lots and/or in garages served by alley access.

DAY CARE CENTER

A school for pre-elementary school age children which provides controlled activities and instruction.

DEMOLITION

The deliberate removal or destruction of the frame or foundation of any portion of a building or structure.

DIRECTOR

The Director of the Community Development or his or her designee, unless otherwise specified.

DWELLING, UNIT

A building or portion thereof intended for occupancy or occupied by one family exclusively, and containing but one kitchen.

EATING AND DRINKING ESTABLISHMENTS

Businesses serving prepared food or beverages for consumption on or off the premises.

FACADE

The face of a building.

FAMILY DAY CARE HOME

A dwelling where day care is provided for children under 18 years of age who are unrelated to the licensee. A small family day care home is for six or fewer children and a large family day care home is for seven to 12 children. Both limitations include the number of children residing in the dwelling unit.

FEASIBLE

Capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social and technological factors.

FORMULA RETAIL BUSINESS

A retail business which, along with seven or more other business locations, is required by contractual or other arrangement to maintain any of the following: standardized merchandise, services, decor, uniforms, architecture, colors, signs or other similar features.

FRONTAGE

The linear length of a building or lot that contains a public entrance. To calculate frontage length, it is measured along the property line adjacent to a street, curb line, or vehicular access easement.

GARAGE

An accessory structure or any part thereof designed or used for parking or storing one or more vehicles.

GARDEN CLUSTER

Single-family attached and semi-attached dwelling units clustered around courtyards, gardens, or green space.

GRADE, (FINISHED)

The lowest point of ground elevation of the finished surface of the ground after any construction or grading activities (including, but not limited to cut and fill of existing slopes) as measured from a known fixed reference height benchmark or as a height referenced from sea level.

GRADE, (NATURAL)

The lowest point of ground elevation of undisturbed soil as measured from a known fixed reference height benchmark or as a height referenced from sea level.

GREEN SPACE/GREEN OPEN SPACE

For purposes of this Specific Plan and calculating open space requirements green space and green open space is grass or landscaped areas. These can include but are not limited to parks, bioretention, common and private residential green space, planters larger than 50 square feet, landscaped planting strips, drivable turf-block, and parking lot landscaping. Trees planted in tree wells shall not be calculated as part of the green space requirement.

GROSS UNIT AREA

- The dwelling unit measured to the outside face of the exterior wall or to centerline of a common wall, if any.
- Basements are included.
- Internal stairways in multi-story units are counted at each floor, except the uppermost floor.
- Exterior spaces useable by the dwelling unit that are unenclosed space such as porches, balconies or terraces are excluded. Screened enclosures are included.
- Outdoor, enclosed storage closets on decks are included.
- Garages or carports are excluded.
- Attic spaces that are not habitable are excluded.
- Elevator shafts are excluded, except at lowest floor.

HARDSCAPE

For purposes of this Specific Plan and calculating open space requirements, hardscape refers to private or common paved areas for the use of pedestrians including plazas, courtyards, pathways, sidewalks and pedestrian paseos. Roads and parking areas shall not be calculated as part of the open space or hardscape requirement.

HEIGHT

The height of all structures, excluding fences, shall be determined by the plumb vertical distance from the natural or finished grade, whichever is lower and creates a lower profile, to the uppermost point of the roof edge, wall, parapet, mansard, or other point directly above that grade. For portions of a structure located directly above a cellar, the height measurement for that portion of the structure shall be measured as the plumb vertical distance from the existing

natural grade to the uppermost point of the structure directly over that point in the existing natural grade. No point of the roof or other structural element within the exterior perimeter of the structure shall extend beyond the plane established by the maximum height plane. Maximum building height includes all elements and height exceptions are not permitted within the Specific Plan Area.

HOTEL

A building where lodging, with or without meals, is provided for compensation and where occupancy is generally limited to no more than 30 days.

LANDSCAPED PARKWAY

A strip of land located between the back of the curb and the front of a sidewalk, usually used for planting low ground cover and/ or street trees - also known as a “park strip”, “planting strip” or “parkway strip”.

LANDSCAPING

An area devoted to or developed and maintained with native or ornamental plantings, lawn, ground cover, gardens, trees, shrubs, or other plant materials. Plants on porches, or in boxes attached to buildings are not considered landscaping.

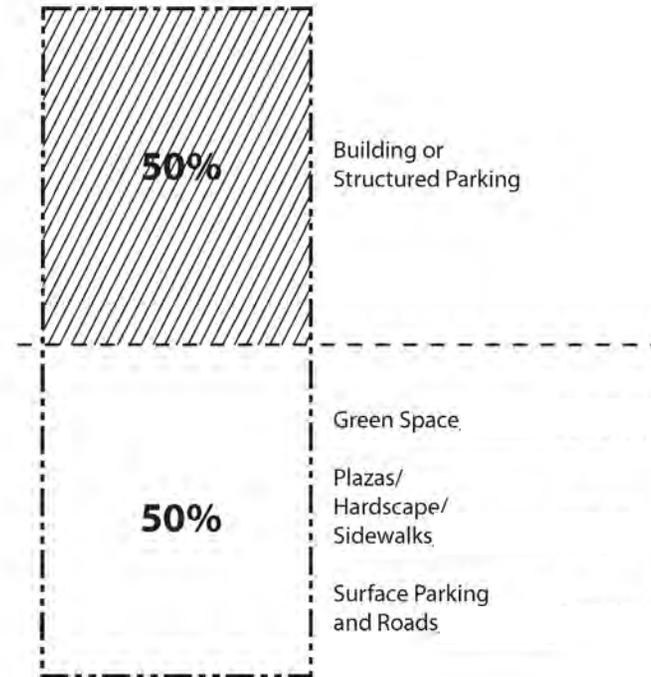
LIVE/WORK

A live/work unit is a dwelling unit or sleeping unit in which a significant portion of the space includes a non-residential use that is operated by the tenant. Dwelling units that include an office that is less than ten percent of the area of the dwelling unit shall not be classified as a live/work unit. If the unit does not comply with the following, it is not considered a live/work unit:

- a. The non-residential area is permitted to be a maximum 50 percent of the area of each live/work unit.
- b. The non-residential area function shall be limited to the first or main floor only of the live-work unit.
- c. A maximum of five non-residential workers or employees are allowed to occupy the non-residential area at any one time.

LOT AREA COVERAGE

Lot Area Coverage is defined as the portion of a lot area covered by the footprint of structures. Coverage shall include all principal and accessory buildings including dwellings, garages, carports, parking structures, greenhouses, enclosed patios, and tool sheds. Gardens or plazas on top of podium parking that are no more than six feet above average finish grade will not count towards Lot Coverage. The maximum height above finished grade will be calculated as an average. Coverage shall not include areas paved at grade for driveways, walkways, roads, green open space (as defined in this Specific Plan), hardscape (as defined in this Specific Plan), uncovered parking, uncovered or unenclosed swimming pools or covered patios provided that said patio is not more than 50% enclosed. The projection of cornices, eaves, balconies, awnings, and other similar architectural projections shall not be included in the calculation of coverage.



LOT COVERAGE DIAGRAM

LOADING AREA, OFF-STREET

A space or berth on the same lot with a building, or contiguous to a group of buildings, for the temporary parking of commercial vehicles while loading or unloading merchandise or materials, and which abuts upon a street or alley or other appropriate means of access.

MARKET HALL/SPECIALTY MARKET

A building housing a market where food and merchandise is sold. Retail sale of food, beverages, primarily for off-premises consumption including delicatessens and other specialty food shops, as well as, establishments which have a sizeable assortment of fresh fruits and vegetables and fresh-cut meat.

MULTI-FAMILY

A building or portion thereof used or designed as a residence for three or more families living independently of each other, including apartment houses, apartment hotels and flats, but not including auto courts

MULTI-FAMILY FLATS

Dwelling units typically stacked one above another with access by way of common building entries and corridors. Parking is usually accommodated in common areas composed of surface parking with carports or individual garages, separate parking structures, or in a parking level located beneath the residential complex (also referred to as Podium Parking defined below).

MULTIMODAL

Multimodal refers to giving travelers more choices than simply using their cars, such as rail and bus transit, carpools, walking, biking, and shuttle service. Multimodal path is for bike and pedestrian use.

NET UNIT AREA

- Net area is the floor area of conditioned space (heated/cooled) measured to the inside face of the exterior walls (or common walls), but including interior partitions.
- Stairs internal to units counted at the bottom floor only.
- Elevator shafts are excluded.

OFFICE

Offices of firms or organizations providing professional, executive, management, or administrative services, such as architectural, engineering, graphic design, interior design, real estate, insurance, investment, legal, and veterinary offices. New medical offices are not permitted.

OPEN SPACE

Open space means a ground plane open and generally unobstructed from the ground plane to the sky. Balconies, shade structures, and roof eaves may extend over a portion of the open space. Open space includes both “green open space” and “hardscape” (plazas, courtyards, pathways, sidewalks, and pedestrian paseos). Plazas, courtyards, and planters over podium parking or on roof decks also qualify as open space.

PARKING LOT LANDSCAPING

For calculations of green space, parking lot landscaping includes all planters 50 square feet or larger, but not trees in tree grates.

PERSONAL SERVICE BUSINESS

Uses that predominately sell personal convenience services directly to the public, including but not limited to, barbers, beauty salons and related services, cosmetologists, electrolysis, facial and/or skin care, hair dressers and/or hair stylists, hair removal and/or replacement, manicurists, nail salons, pedicurists, permanent make-up, skin and body care, piercing, spas, tanning salons, tattooing, cleaners, dog grooming, tailors and other services of a similar nature. Personal service business does not include travel agencies, insurance offices, law offices, architect offices, or any other type of office use.

PODIUM PARKING

A parking structure configured with the parking partially located below grade (but not fully underground), and with dwelling units or other uses above.

PRIMARY BUILDING FRONTAGE

The face of building that faces onto primary streets - Lark Avenue, Los Gatos Blvd, South ‘A’ Street, North ‘A’ Street, Neighborhood Street, Noddin Ave, Burton Rd, and commercial streets within The Northern District.

PRIMARY STREET.

Primary Streets are identified as Lark Avenue, Los Gatos Boulevard, South ‘A’ Street, North ‘A’ Street, Neighborhood Street, Noddin Avenue, Burton Road, and Commercial Streets within the Northern District.

PRIVATE OPEN SPACE

Grass, landscaped area or hardscaped area (patio/balcony), that for exclusive use of an individual unit’s residents and their guests. This space can be either green space or hardscape. For purposes of this Specific Plan, private open space is included in the 30% Open Space requirement.

PUBLIC/PUBLICLY ACCESSIBLE OPEN SPACE

Open space, streets and sidewalks in the North 40 will be privately owned, but publicly accessible. For purposes of this Specific Plan “public” refers to publicly owned, or publicly accessible spaces or uses.

RESIDENTIAL TYPES

Residential types descriptions are in this glossary:

- Cottage Cluster
- Townhomes/Garden Clusters
- Multi-Family Flats
- Condominiums
- Live/Work Lofts

RESIDENTIAL UNIT SIZE MIX EXAMPLE

The Specific Plan encourages a mix of residential types and sizes but does not specify exact sizes. The types and sizes are targeting the unmet needs in Los Gatos. A **hypothetical example** of how the mix of residential uses can be realized is illustrated in the table below. This table is only intended as an example of how a mix of residential uses could be proposed on the North 40. It does not represent a target or requirement.

CONCEPTUAL MODEL OF RESIDENTIAL SIZES					
TYPES	NET UNIT AREA RANGE	GROSS UNIT AREA RANGE	APPROX. UNIT RANGE	PERCENT OF TOTAL RANGE	APPROX. TOTAL AREA
COTTAGE CLUSTER (DETACHED PRODUCT)		1,000 - 1,200 sf	40-50	20-25%	40,000 - 60,000
GARDEN CLUSTER		1,000 - 1,999 sf	40-50	20-25%	40,000 - 60,000
TOWNHOMES, ROWHOUSES		1,000 - 1,999 sf	130 - 140	30 - 40%	130,000 - 280,000
GROSS UNIT AREA TOTAL					210,000 - 400,000
CONDOS/ MULTI-FAMILY	1,300 - 2,350 sf		90 - 110	25 - 30%	117,000 - 258,000
APARTMENTS/ AFFORDABLE	500 - 750 sf		45 - 55	10 - 15%	22,000 - 42,000
MAXIMUM UNITS ALLOWED			364		
NET UNIT AREA TOTAL					139,000 - 300,000

Refer to definitions for Net Unit Area and Gross Unit Area.

Note: 100% is not intended to be achieved by adding the example Percent of Total Range numbers, as it is not required to use every residential product type listed in the table.

RESTAURANT / EATING AND DRINKING ESTABLISHMENTS

A retail food service establishment in which food or beverage is prepared, served and sold to customers for on-site or take-out consumption

RETAIL USE

Providing on site sales directly to the consumer for consumer or household use, including but not limited to the following: small markets/businesses which sell meat, vegetables, dairy products, baked goods, candy and/or other food products (including convenience market), household cleaning and maintenance products, cards, stationary, notions, books, cosmetics, specialty items, hobby materials, toys, household pets and supplies, apparel, jewelry, fabrics, cameras, household electronic equipment, CD music and movies, sporting equipment, kitchen utensils, home furnishings, home appliances, art supplies and framing, art work, antiques, paint, wallpaper, carpet, floor covering, office supplies, musical instruments, hardware, homeware, computers and related equipment/supplies, bicycles, automotive parts and accessories (excluding service and installation), and flowers, plants and garden supplies (excluding nurseries). Retail sales that are incidental to the primary use will not satisfy this definition.

RIGHT-OF-WAY (ROW)

That portion of property that is dedicated, or over which an easement is granted, for public and private streets, utilities, pedestrian access or alleys.

ROWHOUSES

Single-family, attached dwelling units constructed in rows along common streets. Unit entries are oriented to the fronting streets, and garages are integrated into the individual units at the rear. Private open space is typically provided as a porch, entry garden or deck rather than as at-grade patios. Units occupy all floors, without other units above or below.

SCHOOLS

All schools or institutions, whether public or private, and whether or not organized for profit, which give a course of study as defined or determined by divisions of the Education Code of the State.

SETBACK

As defined by “Yard” in this Specific Plan Glossary.

SHARROW

A travel lane with bikes indicated by a sharrow marking on the pavement. This marking is placed within a travel lane to indicate that a bicyclist may use the full lane. The sharrow symbol consists of a bicycle symbol with two chevron markings above the bicycle.

SHUFFLE STALLS

An extra stall to allow for temporary parking for operation of tandem parking spaces.

STORM WATER BEST MANAGEMENT PRACTICES (BMP's)

Methods minimizing the effect of urbanization on site hydrology, urban runoff flow rates or velocities, and pollutant loads.

STORM WATER MANAGEMENT

Storm water management is the practice of “controlling” runoff generated from a storm event to reduce flood potential and other potential negative implications. Types of “control” measures may include underground storm drain systems of pipes, retention basins, infiltration BMP's, pump stations and channels.

STORY

That portion of a building included between the upper surface of any floor and the upper surface of the floor next above, except that the topmost story shall be that portion of a building included between the upper surface of the topmost floor and the ceiling or roof above. If the finished floor level directly above a basement or cellar is more than six feet above grade, such basement or cellar shall be considered a story.

STREET

Any thoroughfare for the motor vehicle travel which affords the principal means of access to abutting property, including public and private rights-of-way and easements.

STRUCTURE

That which is built or constructed, an edifice or building of any kind, or any piece of work artificially built up or composed of parts joined together in some definite manner, including a gas or liquid storage tank that is principally above ground.

TANDEM PARKING

A parking configuration wherein two spaces are located end to end in such a manner that one of the spaces is not directly accessible to the street without traveling over the other space.

TOWNHOMES/GARDEN CLUSTER

Single-family attached dwelling units constructed in clusters within an overall master development plan. Parking is typically in garages or parking lots adjacent to the dwelling unit clusters but may be integrated into the dwelling's ground floor. Townhomes can also be located over podium parking.

UNMET RESIDENTIAL NEEDS

At the time this Specific Plan was drafted, some of the unmet residential needs that have been identified include senior housing, affordable housing, and housing catering to young adults and "empty nesters".

USE

The purpose for which a site or structure is arranged, designed, intended, constructed, erected, moved, altered or enlarged, or for which either a site or a structure is or may be occupied or maintained.

VEHICLE

Automobiles, buses, motorcycles, boats, trailers, mobile homes, recreational vehicles, trucks, campers, motorized construction equipment, tractors and similar devices.

WALL, EXTERIOR

One of the sides of a building connecting foundation and roof. A wall encompasses the total height and width of the side of the building, the exterior or interior wall covering and the studs/structural elements used in the framing of the wall.

YARD

An open space on the same site as a structure, unoccupied by any use except for landscaping, sidewalks, driveways or swimming pools, and unobstructed from the ground upward, unless otherwise specified in any zone, including a front, side or rear yard or space between buildings or structures.